

City of SeaTac

SEPA Environmental Checklist Proposed City Center Park-and-Fly Zoning Code Amendments



August 2, 2012

ENVIRONMENTAL CHECKLIST

Date Checklist Prepared: July/August 2012

Parcel No. The proposed code amendment is applicable to the construction of park-and-fly structured parking projects within the SeaTac City Center Boundary

A. BACKGROUND

1. Name of proposed project:

City Center Park-and-Fly Zoning Code Amendments

2. Name of applicant:

Department of Community and Economic Development, City of SeaTac

3. Address, phone numbers and email of applicant:

Kate Kaehny, Senior Planner

City of SeaTac

4800 South 188th Street

SeaTac, WA 98188

206-973-4750

4. Name of contact person:

Same as above.

5. Address, phone numbers and email of contact person:

Same as above.

6. Agency requesting checklist:

City of SeaTac

7. Proposed timing or schedule (including phasing):

- Planning Commission review anticipated September 2012
- SeaTac City Council review anticipated September 2012
- Public Hearing on proposed Code Amendments at the City of SeaTac anticipated September 2012
- SeaTac City Council action anticipated October/November 2012

8. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

It is currently not anticipated that a future review or amendment to the proposed City Center Park-and-Fly Zoning Code will be required.

9. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

This Environmental Checklist contains the environmental information prepared for this non-project proposal.

It should be noted that, on July 13, 2011, the City of SeaTac prepared an Environmental Checklist and issued a SEPA Threshold Determination of Non-Significance (DNS) for an earlier version of this proposal (see SEP11-00010). The DNS received two appeals and the City subsequently withdrew the Threshold Determination. Compared to the previous Environmental Checklist, this revised Checklist provides additional review of all elements of the environment and is based on an updated proposal consisting of one option for revised regulations instead of the two options, referred to as Model One and Model Two, in the 2011 proposal. The proposal considered in this revised Checklist is based on the option previously identified as Model One. Please see the description of the proposal, below.

As part of the preparation of this Environmental Checklist, the following documents were reviewed and where applicable are referenced in this document:

- City of SeaTac Comprehensive Plan Environmental Impact Statement (EIS), August, 1994
- City Center Plan Draft Supplemental Programmatic Environmental Impact Statement (DSEIS), April 20, 1999
- City Center Final Supplemental Environmental Impact Statement (FSEIS), November 12, 1999
- SMC 15.35 Special Standards for the City Center Determination of Non-Significance (DNS), File No. SEP0012-99
- Parking Bonus Incentive Program Code Amendment (*revising parking bonus formulas*) Determination of Non-Significance (DNS), File No. SEP08-00017
- SEPA and associated studies for the construction of the Wally Park Mixed Use Development (*including structured park-and-fly*), File No. SEP04-00018
- SEPA and associated studies for the Rezone, Development Agreement and construction of a mixed use building known as Master Park (*including structured park-and-fly*), File No. SEP05-00021
- Port of Seattle. Environmental Review/NEPA Environmental Assessment Final Seattle-Tacoma International Airport Comprehensive Development Plan POS SEPA No. 07-09. August 2007.
- Bow Lake Joint-Use Facility Study, June 2000, Edaw, Inc. & EarthTech, Inc.

Any potential future project-level application relating to the construction of a structured park-and-fly parking development may require an applicant to prepare an environmental study as specified in the SeaTac Municipal Code or as requested by the City as part of a SEPA review and application for development approval.

This may include an analysis of traffic impacts, visual impacts, or other pertinent studies.

10. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? Explain.

There are no applications pending for government approvals that would be affected by the proposed amendments. The City has received informal inquiries about potential development, these inquiries may become a formal proposal in the future.

11. List any government approvals or permits that will be needed for your proposal.

- Pursuant to RCW 36.70A.106, the Washington State Department of Community, Trade and Economic Development has completed its 60 day review of the proposed code amendments. No comments were received in relation to the proposed changes.
- Pursuant to RCW 36.70.580, a public hearing will be held by the SeaTac Planning Commission. At the time of preparing this evaluation, a date has not been specified.
- The proposed code amendments will be presented to City Council for action. Council provides a recommendation for adoption of the proposed code amendments as follows; adopt as prepared, request changes to the Ordinance or vote to not adopt the Ordinance.

12. Give brief, complete description of your proposal, including the proposed uses and the size, with square footage, of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.

This is a non-project proposal for proposed code amendments to SMC Title 15, the SeaTac Zoning Code. The proposed standards amend the existing development and design standards for park-and-fly parking structures, and any projects that include park-and-fly parking structures, within the SeaTac City Center boundary.

These proposed amendments, if approved, are intended to implement the SeaTac Comprehensive Plan and City Center Subarea Plan by promoting enhanced building quality, a more pedestrian-oriented environment and the economic feasibility of projects. The proposed amendments do not include any changes to the fundamental parking use or zones where parking is allowed in the City Center. Key changes include:

1. Increase in the maximum number of permitted stalls from 300 stalls in the existing standard to 1,000 stalls in the proposed amendments.
2. Re-incentivizing the Parking Bonus Incentive Program, while still encouraging the provision of public benefits, by proposing changes to four of the five bonus formulas.

3. Changes to the vehicular circulation requirements to reference SMC Title 11 (Streets, Sidewalks, and Public Thoroughfares) rather than the existing City Center-specific standard.
4. No changes are proposed to the dimensional or design requirements for “Front Yard Open Space” but the overall percentage of “Net Site Area” open space to be provided has been reduced from 10% to 5%.
5. Enhancing building modulation requirements to mitigate the scale of structures, and increased building façade treatments such as architectural focal points;
6. Adding requirements for the screening of parking; and
7. Increasing the amount of ground floor commercial space required when projects are constructed immediately adjacent to the street.

Tables 1 and 2 provide a summary of the key proposed amendments. The complete text of all proposed amendments is included as Attachment 1 to this Environmental Checklist.

Table 1. Development Standards: Comparison of Existing Code and Proposed Amendments

Development Standards		
Development Standards	Existing Code	Proposed Code
Stalls	300 Base number	1000 Base number
Parking Structure Per Development Site	<ul style="list-style-type: none"> a) 1 per development site. b) No stand-alone parking structure permitted on development site created through commercial/industrial subdivision. 	<ul style="list-style-type: none"> a) <i>No proposed change to standard</i> b) <i>Remove standard</i>
Building Lot Coverage	75% building lot coverage	<i>No proposed change to standard</i>
Building Orientation	Front façade/s of primary building onsite shall be oriented toward the front property line/s, with the main pedestrian entrance located on this façade (SMC 15.35.310B)	<i>No proposed change to standard</i>
Vehicle Circulation	If developer constructs a street, it is to be as per code requirements SMC 15.35.200 <u>Road Design</u> <ul style="list-style-type: none"> a) Connection to adjacent parcels required where possible b) 48' wide collector streets c) 60' minor arterials d) 400' maximum block width e) Co-ordination with adjacent property owners f) Avoid dead-end streets g) No enclosure of streets 	<i>Remove standard and replace with SMC Title 11 Streets, Sidewalks, and Public Thoroughfares</i>
Pedestrian Circulation (on site sidewalks)	4' pedestrian sidewalks/pedestrian connections are required to connect pedestrians to off-site pedestrian walkways, as per code requirements SMC 15.35.220	<i>No proposed change to standard</i>

Development Standards		
Development Standards	Existing Code	Proposed Code
Public Open Space - Amount of Open Space	<p>a) 10%: Minimum of 10% of net site area to be permanent outdoor useable open space.</p> <p><u>Open Space Design</u></p> <p>b) Code has specific front yard open space requirements as to how open space is designed.</p> <p>I. 5 feet x length of street facing front façade is required as front yard open space (SMC 15.35.400s)</p> <p>II. 200 SF plaza, courtyard, pocket park with seating and focal point: art, water feature, transit stop, stage area, accessory site furnishing</p> <p>III. Multi purpose green space-grass pedestrian ways and seating</p> <p>IV. 5' decorative paving</p>	<p>a) Reduce net site area from 10% to 5%</p> <p>b) No proposed change to standard</p>
Landscaping	<p>a) Street Frontage: Front yard landscaping is superseded by open space requirement (SMC 15.14.020)</p> <p>b) 5' Building Façade: 5 feet (not required if there is weather protection which is required for ground floor commercial uses)-SMC 15.35.930 description of landscape strip contents</p> <p>10' Side/Rear: 10 feet</p> <p>buffer to non compatible zones: 20 feet</p>	<p>No proposed changes to standards</p>
Streetscape Design	<p>Total - 12' wide pedestrian zone (SMC 15.35.335)</p> <p>a) 8' Sidewalk Clear-Through Zone-8' along public and/or private street frontage</p> <p>b) 4' Street landscaping zone- 4' wide landscaping zone adjacent to street curb, including trees, landscaping, light poles, street furniture</p>	<p>No proposed change to standard</p>
Public Art	<p>Currently public art is not required but is an option under the bonus incentive program.</p>	<p>No proposed change to standard</p>

Development Standards		
Development Standards	Existing Code	Proposed Code
Ground Floor Depth Of Commercial Space	20' deep from exterior parking structure façade which can be averaged, with no less than 15'	<i>30' minimum depth</i>
Ground Floor Commercial Ceiling Height	10' minimum clear ceiling height for ground floor uses	<i>Remove standard. No minimum ceiling height is required</i>
International Boulevard Façade Treatment of Second Story	No Standard	<i>The first two stories of the building are to have the appearance of a retail/commercial/office building</i>
Transparency (Windows)	<ul style="list-style-type: none"> a) 60%: Windows to cover 60% of public street facing ground floor building area to finished ceiling height, b) begin 12" -30" above finished grade c) Applicability: Transparency applied to buildings with ground floor retail/commercial uses including portion of buildings where ground floor uses are convertible (SMC 15.35.510) 	<i>No proposed change to standard</i>
Weather Protection	<ul style="list-style-type: none"> a) Weather protection along at least the length of the façade of ground floor retail/commercial uses (SMC 15.35.570) b) 4' minimum depth c) 8' maximum or 75% of distance to curb face d) Architecturally integrated e) 8'6" minimum height 	<i>No proposed change to standard</i>
Building Height	CB-C Zone: FAA regulated	<i>No proposed changes to maximum building heights. Change proposed to require one story structures to be a minimum of 18 feet and have the appearance of two story structures.</i>

Development Standards		
Development Standards	Existing Code	Proposed Code
Vertical Façade Treatments	<p>Applicability: All street facing and non street facing facades over 150' in length</p> <p>Vertical Façade Changes: Choose 1 (min): Required every 80' such as:</p> <ol style="list-style-type: none"> 1. variation of garage floor openings, 2. changes in architectural materials, 3. projecting forward or recessing façade elements (SMC 15.35.920) 	<p><i>Requirement reduced to every 40' for street facing facades. and no change in vertical façade treatments for non-street facing facades</i></p>
Horizontal Façade Treatments	<p>Applicability: All street facing and non street facing facades in length</p> <p>Horizontal Façade Changes: Choose 1 (min): Incorporate:</p> <ol style="list-style-type: none"> 1. upper floor step back, 2. material changes base and upper floors, 3. cornice line or weather protection (SMC 15.35.920) 	<p>Applicability: STREET FACING:</p> <p>Horizontal Façade Changes: Choose 1 (min): Incorporate:</p> <ol style="list-style-type: none"> 1. No proposed change to standard 2. No proposed change to standard 3. another architectural element. <p>Applicability: NON STREET FACING Maintain existing standard EXCEPT applicability now applies only to facades greater than 150'.</p>
Architectural Focal Point on Front Façade	No standard	<p><i>Required on any building façade that can be seen from public view near the main activity areas, building entrances, pedestrian corners. Choose 1 of the following (4) options:</i></p> <ol style="list-style-type: none"> 1. unique materials 2. height differentiation 3. tower elements or roof lines to accentuate corner 4. variation in building massing

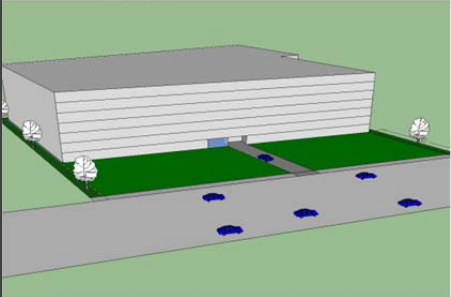
Development Standards		
Development Standards	Existing Code	Proposed Code
<p>Corner Lots:</p> <p>Architectural Focal Point on Front Facade</p>	<p>Corner Lots Orient both facades to both the corner and adjacent public and/or private streets to increase visibility and landmark status-choose 1: a) transparent glazing b) tower elements or roof lines to accentuate corner c) balconies or terraces at corner (SMC 15.35.320):</p>	<p><i>Required on corner lots at intersections of I.B and non I.B streets. Choose 1 of the following (6) options:</i></p> <ol style="list-style-type: none"> 1. <i>orientation of the building</i> 2. <i>unique materials</i> 3. <i>architectural features</i> 4. <i>tower elements or roof lines to accentuate corner</i> 5. <i>height differentiation</i> 6. <i>variation in building massing</i>
<p>Screening Of Parking Structure: At Grade</p>	<p>Facades facing a public street or pedestrian ways <u>Screening of at grade Parking:</u> a) Choose 1 method: 1) decorative trellis work and/or screening or 2) Glass window display cases (SMC 15.35.930) AND b) Parking must be completely enclosed or wholly screened through any combination of walls, decorative grilles, or trellis work with landscaping (SMC 15.35.940B)</p>	<p><i>Completely enclosed or screened through any combination of walls, grilles, or transparent or opaque glazing.</i></p>
<p>Screening Of Parking Structure: Upper Floors</p>	<p>Facades facing a public street or pedestrian ways <u>Screening of Upper Floor Parking:</u> choose 1 method: 1) planters integrated into upper floors, 2) decorative trellis work and/or screening or 3) design window like openings on facade (SMC 15.35.930)</p>	<p><i>Facades facing a public street or pedestrian ways</i></p> <p><u><i>Screening of Upper Floor Parking:</i></u> <i>Incorporating a combination of decorative grilles, railings, the design of window like openings, and or/other screening materials.</i></p>
<p>Prohibited Screening Materials</p>	<p>No standard</p>	<p><i>Prohibit reflective materials, corrugated fiberglass and chain link fencing</i></p>

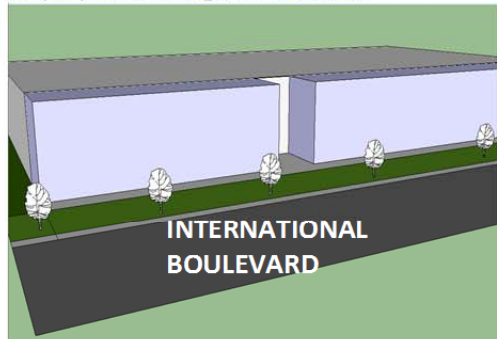
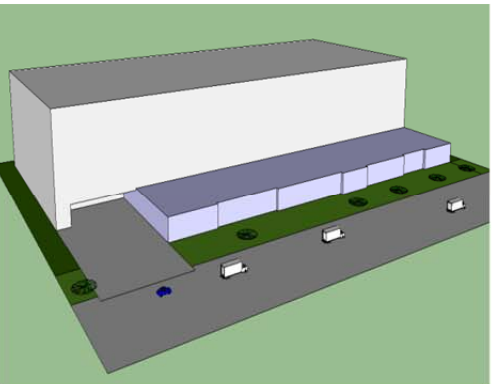
Development Standards		
Development Standards	Existing Code	Proposed Code
Roofline Variation	Parking Structure Top Floor Wall Design-Choose 1: a) Top floor wall with architectural focal point b) wall line variation-projecting cornice c) articulated parapet –angled, curved or detail elements(SMC 15.35.910D)	<i>No proposed change to standard</i>
Building Materials	No standard	<i>Quality durable materials shall be used. Predominant materials shall include any combination of brick, wood, stone, and or tinted/textured concrete</i>
Blank Wall	a) 50' or 20% of façade: Blank walls not to exceed 50' in length or 20% of length of street facing façade, whichever is less. b) Required to select 1 (of a choice of 3) or more design treatments for the ground floor: 1)trellis work, 2)decorative masonry pattern, 3)small setbacks and projections. (SMC 15.35.530)	<i>No proposed change to standard</i>
Pedestrian Building Entrance	Primary entrance shall be no more than 3' and clearly visible through: choose 2 or more: a) Canopies, awnings, entry overhangs b) Variation in building footprint, roof form, transparent glazing c) Lighting less than 16' height d) Focal point landscaping	<i>No proposed change to standard</i>
Parking Entrances	Garage entrance less prominent than pedestrian entrance (SMC 15.35.940A)	<i>No proposed change to standard</i>

Development Standards		
Development Standards	Existing Code	Proposed Code
CPTED Design Requirements	Total of 10 standards listed and are based on the IESNA Handbook. (17.28.030) a) Recommended stairs and elevators located adjacent to street b) Walkways elevated c) Hiding spaces below stairs closed off d) Directional arrows required e) Exterior stairway doors one way locks – exit but not enter f) Pedestrian entrances centralized to one portal g) Ground level pedestrian exits to non secure areas – emergency exit only h) CCTV and emergency phones recommended i) Fencing used only for security. 6’ black chain linked fence j) Landscaping not located within 3’ of permitted curbing of permitted of parking lot k) Lighting requirements	Maintain existing standards with following changes and additions: a) Remove Standard: CPTED requirements exempt for developments where “attendants park cars”. Remove this standard and apply to all development. (Police recommendation) b) Stairwells: Recommended stairs and elevators shall be located adjacent to street and 100% transparent from the street (police recommendation) c) Elevators: shall be located at the exterior of the building, preferably on street side, and oriented so elevator lobby visible from street at each level. Back of elevator cab and shaft shall be made of glass to allow for maximum surveillance d) Restrooms: In parking garages without staffing, public restrooms should be avoided. If the inclusion of a public restroom is unavoidable, then they shall be placed so that the doors are visible from the attendant’s normal working position. The bathrooms shall have a “labyrinth” entrance that allows cries for assistance to be heard. Panic alarms and motion-activated lighting shall also be installed.
Right Of Way (ROW)	1 stall per 150 sf Dedicated public right of way (ROW) in accordance with City Center vehicular and pedestrian access plan	1 stall per 100 sf of ROW
Open Space	1 stall per 150 sf Publicly accessible open space in excess of required 10%. Contributions to open space fund can also be accepted in lieu of open space	1 stall per 100 sf of Open Space
Commercial/Service/Residential Space	1 stall per 250 sf Provide additional retail/commercial/services/residential space (excluding hotel/motel uses) in addition to code required commercial space, included at the time of construction	1 stall per 25 sf

Development Standards		
Development Standards	Existing Code	Proposed Code
Hotel/Motel Unit	0.5 stall per hotel/motel unit Provide a hotel/motel unit in addition to minimum ground floor retail/commercial space	<i>1 stall per hotel/motel</i>
Water Feature/Public Art Display	60 additional stalls 1 water feature or public art display of equivalent value incorporated into open space. formulae identified in standard	<i>60 stalls per art/water feature provided</i>

Table 2. Setbacks and Commercial Space Requirements: Comparison of Existing Code and Proposed Amendments

Setbacks and Commercial Space Requirements			
	Building Type	Existing Standard	Proposed Standard
Option 1	Building Type One – Fronting International Boulevard	100' setback to parking structure with 50% ground floor commercial to be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for park-and-fly office uses)	<i>The amount of commercial space is reduced due to economic considerations of accommodating a 100' setback.</i>
			<i>100' setback to parking structure with 400 sf commercial space (which can be used for park-and-fly office uses)</i>
			

Setbacks and Commercial Space Requirements			
	Building Type	Existing Standard	Proposed Standard
Option 2	Building Type Two – Fronting International Boulevard	0'-20' setback (for at least 50% of the buildings front façade) to commercial development and no setback to parking structure with full height commercial building to screen parking structure	<p><i>No proposed change to standard.</i></p> 
Option 3	Building Type Three– Fronting International Boulevard	0'-20' setback (for at least 50% of the buildings front façade) to commercial development; 100' setback to parking structure; 50% ground floor commercial be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for -fly office uses)	<p><i>The setback to the commercial space is reduced and the amount of commercial space required has been increased.</i></p> <p><i>0'-20' setback to commercial development; 75' setback to parking structure; 75% ground floor commercial available at time of occupancy</i></p> 

Setbacks and Commercial Space Requirements			
	Building Type	Existing Standard	Proposed Standard
Option 4	Fronting A Non International Boulevard Street	0'-10' setback (for at least 50% of the buildings front façade) ; 50% ground floor commercial be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for park-and-fly office uses)	<i>Maximum setback is removed and commercial space required has been reduced.</i> <i>10' min. setback to commercial development and parking structure;</i> <i>15% (not to be less than 400 sf) ground floor commercial space required at time of occupancy</i>
Option 5	Fronting a Non International Boulevard Street and is a Corner Lot with International Boulevard (I.B.).	I.B Setback: 0'-20' setback (for at least 50% of the buildings front façade) to commercial development; I.B Commercial Space: 50% ground floor commercial be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for park-and-fly office uses); Non I.B. Setback: 0'-10' setback (for at least 50% of the buildings front façade); Non I.B. Commercial Space: Commercial space required for the first 100' from I.B	<i>Along I.B. the applicant selects a model from Option 1, 2 or 3. The non I.B. street maximum setback is removed and commercial space required has been reduced.</i> <i>I.B. Setback: applicant selects a setback and commercial space requirement from the three I.B. options available.</i> <i>Non-I.B. Setback:</i> <i>10' min. setback to commercial development and parking structure;</i> <i>20% (not to be less than 400 sf) ground floor commercial space required at time of occupancy</i>

13. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, and section, township, and range. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

The proposed code amendments apply to the City Center boundary as described below and shown in Figure 1.

Description of City Center Boundary: East of International Boulevard within the City. The eastern boundary is approximately 150 feet east of 32nd Avenue South in the northern half and approximately 300 feet east of 38th Avenue South in the southern portion. The northern boundary is 150 feet north of 166th Avenue South, and the southern boundary varies, but lies generally along South 188th Street. Seattle-Tacoma International Airport is west of International Boulevard and forms the western boundary of the City Center area. The SeaTac City Center is located within Sections 27, 28, 33, and 34, Township 23 N, range 4 E, in King County, Washington.

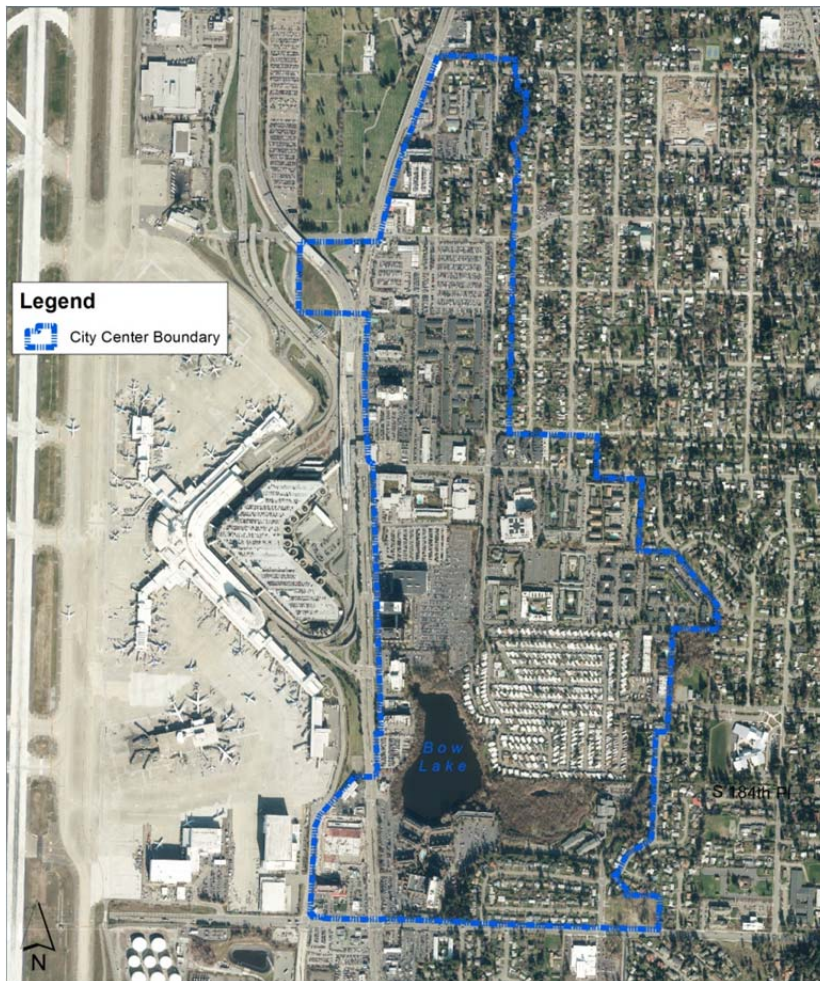


Figure 1. Vicinity Map

B. ENVIRONMENTAL ELEMENTS

1. Earth

- a. General description of the site (article one): Flat, rolling, hilly steep slopes, mountainous, other _____.**

Although generally flat, the City Center area generally slopes down from east to west. The land immediately east of International Boulevard is flat and the terrain slopes steeply up behind these parcels.

- b. What is the steepest slope on the site (approximate percent slope?)**

Steep slopes are found in the following areas:

- Immediately west of 32nd Avenue South between South 176th and South 180th Streets, including an area that has a slope in excess of 40%; and
- West of 38th Avenue South in the southern part of the City Center.

- c. What general types of soils are found on the site (for example – clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland.**

Surface soils have been largely altered and are classified as “Urban” in the King County Soil Survey.

Underlying soils are of the Alderwood-Everett classification, formed under glacial conditions. In some places these gravelly sandy loam soils have underlying layers of dense glacial till. This till can prevent downward movement of water through the soil, allowing water to accumulate in sand gravel lenses at varying depth from the surface.

Soils around Bow Lake are poorly drained and have a seasonal high water table. Peat has been observed during drilling and sampling in the area immediately north of Bow Lake. Ground water is generally nearer the surface in the lower elevations of the study area than in the more elevated, eastern and northern residential areas.

- d. Are there surface indications or history of unstable soils in the immediate vicinity? Describe.**

Landslide/Erosion Hazards. No known landslide or erosion hazards exist in the affected geographic area.

Seismic Hazards. A seismic hazard area has been defined around Bow Lake, extending north/south as well as to the east, immediately south of the Bow Lake Mobile Home Park. The southern edge of the mobile home park is built on imported fill. Such areas require special building and design measures to reduce the potential for seismic damage.

- e. Describe the purpose, type and approximate quantities of any filling or grading proposed. Indicate source of fill.**

As a non-project action, the proposal does not propose grading or filling.

Future site-specific development applications could propose fill and/or grading and will be reviewed consistent with the City of SeaTac Municipal Code. Specific requirements include:

- Commercial development applications in excess of 4,000 square feet of gross floor area must prepare a project-specific SEPA Checklist that includes identification of any unstable soils are present on the site and any proposed mitigation. (WAC 197-11-800(1b(iii)))
- As part of an application for building approval and civil plan review, a geotechnical report prepared by a certified engineer is generally required by the City.

f. Could erosion occur as a result of clearing, construction, or use? Generally describe.

As a non-project action, the proposal would not result in erosion.

Future site-specific applications will be reviewed consistent with the City of SeaTac Municipal Code. Specific requirements include

- Submittal of erosion control plans for review and approval for any proposed clearing and construction permits. (SMC Chapter 12.10)
- An approved Temporary Erosion Sediment Control is required for any proposed grading within the City to mitigate and prevent erosion associated with construction works. All erosion control measures are subject to a minimum of four City inspections and approval during construction activities. (SMC Chapter 12.10)

g. About what percent of the site will be covered with impervious surfaces after project construction (e.g. asphalt and buildings)?

The proposed code amendments do not propose any changes to existing lot coverage standards as specified in the adopted SeaTac Municipal Code, Title 15.

h. Proposed measures to reduce or control erosion, or other impacts to the earth.

- As a non-project action, the proposal is not expected to result in impacts to the earth. No mitigating measures are proposed.
- Future site-specific development proposals will be reviewed pursuant to SEPA, the City's Environmentally Sensitive Areas requirements (SMC Chapter 15.30), Environmental Rules/Procedures (SMC 16A.23), and Surface and Stormwater Management requirements (SMC 12.10).

2. Air

a. What types of emissions to the air would result from the proposal (i.e. dust, automobile, odors, industrial wood smoke) during construction and when the project is completed? Generally describe and give approximate quantities if known.

As a non-project action, the proposal will not directly result in impacts to air quality. Indirectly, the proposal could affect air quality in the following ways:

- Construction activity related to new development could have temporary impacts on air quality including emissions from construction vehicles, increased suspended dust

and smoke particulates during earth-moving activities and from unfinished roads, and additional construction-related traffic. A future project could include demolition work involving asbestos-containing materials and would be assessed by the City at the time of application submittal. The potential for construction phase impacts currently exists under the existing code and it is not anticipated that potential future construction under the proposed amendments would result in a significantly higher level of impact.

- Traffic associated with new development will generate vehicle emissions. Potential emissions that could result from any new or redeveloped park-and-fly projects would be similar to emissions associated with non- park-and-fly garage developments already allowed in the City Center that are of the same scale. Because new park and fly projects would serve air passengers at the Seattle-Tacoma International Airport, it is likely that the majority of traffic associated with any particular garage would be trips diverted from other routes rather than new trips. Over time, traffic associated with park and fly facilities is likely to fluctuate with air passenger activity. Please see Question 14 of this Checklist for additional discussion.
- Greenhouse gas emissions associated with development and operations of new park-and-fly facilities would include embodied emissions associated with construction phase materials and processes and operational phase energy usage and vehicle trips.

Traffic analysis completed as part of the City Center Plan Supplemental Environmental Impact Statement (SEIS) was inconclusive in regards to potential air emissions from build out of the City Center because of the lack of certainty about many aspects of the transportation system which are still unresolved today, such as the likelihood of improvements to SR-509.

b. Are there any off-site sources of emissions or odor that may affect your proposal? Generally describe.

There are no known off-site sources of emissions or odors that are likely to impact the proposed non-project proposal or future project-level development of park-and-fly facilities. The proposed code amendments would not affect the potential for off-site emissions or odor to impact future development.

National Ambient Air Quality Standards (NAAQS) for the Central Puget Sound area, which includes the SeaTac City Center, establish the following:

- *Ozone & Carbon Monoxide*: In 2004, the EPA approved the Central Puget Sound Area 2nd 10-year CO/ozone maintenance plan.
- *Inhalable Particulate Matter (PM10)*: The City Center is a PM10 attainment area for PM10, which pertains to fuel combustion sources like residential wood burning, motor vehicle engines and tires, and other sources.
- *Fine Particulate Matter (PM2.5)*: Except for a portion of Pierce County, it is likely that the Central Puget Sound region complies with federal standards.

According to the December 2001, City of SeaTac and Port of Seattle Joint Transportation Study (JTS), air operations at the Seattle-Tacoma International Airport, which lies adjacent to and immediately to the west of the City Center, contribute to background

conditions. Taken as a single source, however, they do not result in air quality standards exceedence.

c. Proposed measures to reduce or control emissions or other impacts to the air;

As a non-project action, the proposal is not expected to result in impacts to air quality. No mitigating measures are proposed.

Future project-specific development proposals will be reviewed pursuant to SEPA and the City of SeaTac Environmental Rules/Procedures, SMC Chapter 16A.23.

3. Water

a. Surface

1) Is there any surface water body on or in the immediate vicinity of the site (including year round and seasonal streams, saltwater, lakes, ponds, or wetlands?) Describe type and provide names. If appropriate, state what stream or river it flows into.

Bow Lake is the only significant water body within the City Center area. Bow Lake is a palustrine, open water wetland and approximately 30 acres are considered wetland.

Bow Lake retains groundwater and receives runoff from roughly 407 acres of catchment, most of which is paved. A series of catch basins collect stormwater from the surrounding streets and pipes convey the water to Bow Lake. In this way the lake serves as a detention facility for the surrounding basin. Drainage from the lake is to the southwest, where a 36-inch pipe conveys water under both International Boulevard and 28th Avenue South. This pipe surfaces west of the street end for South 192nd Street and becomes the east fork of Des Moines Creek.

2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters. Please describe and attach available plans.

As a non-project action, the proposal does not include any work over, in or adjacent to Bow Lake. The proposed development standards promote Bow Lake as a focal point and encourage recreational uses (trails, paths, etc.) around the Lake's perimeter.

Future site-specific development proposals located within 200' of Bow Lake will require a SEPA review and approval by the City in accordance with review under SMC 15.30. In addition, future site-specific development proposals will be required to follow the 2009 King County Stormwater Design Manual which has been approved by Department of Ecology to meet State water quality and quantity standards (SMC Chapter 12.10). Significant adverse environmental impacts are not anticipated for future projects provided the Design Manual is followed.

3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Please see the reply to Question 3.a.2, above.

Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code, and state and federal requirements.

4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

Please see the reply to Question 3.a.2, above.

Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code, and state and federal requirements.

5) Does the proposal lie within a 100-year floodplain? Note location on the site plan.

The City Center area does not lie within a 100 year floodplain.

6) Does the proposal involve any discharges of waste materials to surface waters? Describe the type of waste and anticipated volume of discharge.

Please see the reply to Question 3.a.2, above.

Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code, and state and federal requirements. As required by the City through SMC Chapter 12.10, potential discharges to surface waters must meet the requirements of the 2009 King County Surface Water Design Manual.

b. Ground

1) Will ground water be withdrawn, or will water be discharged to ground water? Give general description, purpose, and approximate quantities.

As a non-project action, the proposal will not withdraw or discharge to groundwater.

Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code, and state and federal requirements. As required by the City through SMC Chapter 12.10, potential discharges to surface waters must meet the requirements of the 2009 King County Surface Water Design Manual.

2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: domestic sewage; industrial containing the following chemicals; toxic or non-toxic, agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

Please see the response to Question 3.b.1, above.

Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code, and state and federal requirements. As required by the City through SMC Chapter 12.10, potential discharges to surface waters must meet the requirements of the 2009 King County

Surface Water Design Manual. Any untreated stormwater runoff generated from inside the parking structure will likely discharge to the City's sanitary sewer system, where it will be treated prior to discharge.

c. Water Runoff (including storm water)

1) Describe the source of runoff (including storm water) and method of collection and disposal. (include quantities). Where will this water flow? Will this water flow into other waters? Describe.

As a non-project action, the proposal will not result in water runoff.

Any potential future development of a parking structure will generate stormwater runoff. Runoff treatment and detention would be designed to meet the 2009 King County Surface Water Design Manual requirements (SMC Chapter 12.10). Any untreated stormwater runoff from inside the parking structure will likely discharge to the City's sanitary sewer system, where it will be treated prior to discharge. Outside surface runoff will discharge to a City approved designed water quality facility. This will be assessed at the time of a building permit.

2) Could waste materials enter ground or surface waters? Generally describe.

Please see the response to Question 3.c.1, above.

Any potential future development will require the applicant to obtain Site Engineering Approval for drainage. A proposal would be required to meet the standards identified in the King County Surface Water Design Manual, 2009 (SMC Chapter 12.10).

d. Proposed measures to reduce or control surface, ground, and runoff impacts, if any.

As a non-project action, the proposal is not expected to result in impacts to surface water, groundwater or water runoff. No mitigating measures are proposed.

Any potential future development will require the applicant to obtain a Site Engineering Approval for drainage. A proposal would be required to meet the standards identified in the King County Surface Water Design Manual, 2009 (SMC Chapter 12.10).

4. Plants

a. Check or circle types of vegetation found on the site:

deciduous tree: alder, maple, aspen, other

evergreen tree: fir, cedar, pine, other

shrubs

grass

pasture

crop or grain

wet soil plants: cattail, buttercup, bullrush, skunk cabbage, reed canary, other

water plants: water lily, eelgrass, milfoil, other

___ other types of vegetation

The City Center area is largely developed and does not have significant areas of vegetation except for the area within the Class I wetland adjacent to Bow Lake. According to the Bow Lake Joint-Use Facilities Study, completed in June 2000, the Bow Lake Wetland Complex is composed of a variety of wetland habitat types including scrub-shrub, forested wetlands, and open water. Of the 22.5 acre total wetland complex, 9.01 acres are either scrub-shrub or forested wetlands. Documented plant species observed, or likely to occur, in the area include: redtop (*Agrostis alba*), tall fescue (*Festuca arundinacea*), hardhack (*Spirea douglasii*), red-osier dogwood (*Cornus stolonifera*), willow species (*Salix sitchensis* and *S. lasiandra*), velvet grass (*Holcus lanatus*), creeping buttercup (*Ranunculus repens*), black cottonwood (*Populus balsamifera*), red alder (*Alnus rubra*), and slough sedge (*Carex obnupta*). The existing shrub-scrub and forested wetlands are said to be degraded due to land use impacts associated with adjacent urban development.

b. What kind and amount of vegetation will be removed or altered?

As a non-project action, the proposal will not remove or alter vegetation. In addition, the proposal does not include any proposed changes to landscape requirements for park and fly facilities.

Future project specific development proposals that may remove or alter vegetation will be reviewed consistent with the provisions of the SeaTac Municipal Code. Any potential future redevelopment that proposes the removal of vegetation will be required to obtain a clearing permit from the City.

c. List threatened or endangered species known to be on or near the site.

There are no known endangered or threatened species located within the City Center subarea.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site.

As a non-project action, the proposal is not expected to result in impacts to vegetation. No mitigating measures are proposed.

Any potential future construction of a park-and-fly parking structure will be required to satisfy the landscaping requirements of the SeaTac Municipal Code. Landscaping will be required to the front, side, rear and façade of the building. The City encourages the preservation and incorporation of existing vegetation onsite as part of an integrated site design.

5. Animals

a. Circle any birds and animals which have been observed on or near the site or are known to be on or near the site:

City Center is a highly urbanized area with wildlife typical of an urban area. Within the City Center, the following birds and animals have been observed:

birds: hawk, heron, eagle, songbirds, other crows, pigeons

mammals: deer, bear, elk, beaver, other rodents, rabbits

fish: bass, salmon, trout, herring, shellfish, other _____

b. List any threatened or endangered species known to be on or near the site.

There are no known endangered or threatened species located within the City Center area.

c. Is the site part of a migration route? Explain.

The City of SeaTac is located within the Pacific Flyway. It is not anticipated that the construction of a parking structure will impact any migration routes.

d. Proposed measures to preserve or enhance wildlife.

As a non-project action, the proposal will not impact wildlife. No mitigating measures are proposed.

6. Energy and Natural Resources

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

As a non-project action, the proposal will not directly result in any additional need for energy. Future specific developments may use electric, natural gas or other energy sources.

b. Would your project affect the potential use of solar energy by adjacent properties? Generally describe.

As a non-project action, the proposal would not affect solar energy.

c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts.

As a non-project action, the proposal does not directly impact energy consumption. No mitigating measures are proposed. The proposed code amendment does not specify any requirements for the provision of energy efficient conservation measures.

7. Environmental Health

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? Describe.

As a non-project action, the proposal is not expected to result in environmental health hazards. The proposed amendments do not include changes that are expected to increase the potential for environmental health hazards with future development activity.

Depending on specific circumstances, the potential for environmental health hazards impacts associated will be assessed on a project by project basis at the time of application.

1) Describe special emergency services that might be required.

No specific emergency services are required for this non-project proposal. To the extent that the increased number of parking stalls leads to taller buildings, ladder trucks to serve these facilities may be required. However, because the City Center does not limit height (except for FAA requirements), this is not a significant change from existing conditions.

Any unique needs associated with a specific development proposal will be assessed on a project by project basis at the time of application.

2) Proposed measures to reduce or control environmental hazards.

As a non-project action, the proposal will not result in increased environmental health hazards. In addition, the proposed code amendments do not contain measures that are likely to increase the potential for environmental health hazards at the project-specific level.

Future project-specific development proposals will be reviewed pursuant to SEPA and the City of SeaTac Environmental Rules and Procedures (SMC 16A.23).

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

The City Center is a highly urbanized area. Traffic noise within the City Center is mainly generated by vehicle traffic, and is almost entirely confined to International Boulevard where AM and PM peak hour traffic has the greatest impact. While adjacent to the Seattle-Tacoma International Airport, according to the City of SeaTac Comprehensive Plan Noise Contour Map (Map 1.3), the City Center is outside of the recorded 75 Day-Night Average Sound Level (Ldn) noise contour (area within this contour is most affected by airport noise).

2) What types and levels of noise would be created by or associated with the project on a short time or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

As a non-project action, the proposal is not expected to create noise. The potential for development of park-and-fly facilities is consistent with the Comprehensive Plan and existing development code. Future development is generally not expected to be associated with any unusual noise sources and would be consistent with the nature of existing uses in the surrounding area.

In the future, as project-specific development occurs, construction activity could result in temporary, localized noise impacts.

Future development would also add traffic, incrementally increasing background traffic noise. Compared to an hourly or daily parking structure where traffic volumes per stall are typically higher due to a more frequent turnover of vehicles entering and exiting the facility, a park-and-fly facility is more likely to have longer term parking usage and correspondingly lower traffic generation rates.

The level and type of noise generated would be consistent with the construction and operation of a parking structure. It is not anticipated that the proposed code amendment would result in any additional adverse noise than what is already permitted by the types of uses currently allowed within the City Center.

Construction hours of operation will likely be a condition of permit approval by the City.

3) Proposed measures to reduce or control noise impacts.

As a non-project action, the proposal would not directly impact noise levels. No mitigation is proposed.

At the project level, noise will be minimized by utilizing the following provisions:

- Park-and-fly parking structures are permitted within commercial zones only.
- Proposed setbacks on International Boulevard are consistent with the commercial building setbacks currently required in the City Center, and setbacks proposed on side streets are increased.
- During the construction of the project, the construction hours of operation will be restricted to minimize any adverse noise impacts and administered through a condition of permit approval by the City.

8. Land and Shoreline Use

a. What is the current use of the site and adjacent properties?

Within the City Center boundary, existing uses include several hotels, two park-and-fly parking structures, surface parking lots, retail and commercial office buildings, restaurants, apartment complex developments, single family dwellings and the Sound Transit SeaTac/Airport Link Light Rail Station. To the west, the area is generally bounded by the Seattle-Tacoma International Airport and commercial uses. In other directions, the City Center area adjoins low density residential development and small commercial areas.

b. Has the site been used for agriculture? Describe.

The City Center boundary is a highly urbanized area and has not been used for agriculture in recent history.

c. Describe any structures on the site.

Existing development within the City Center boundary includes: several hotels, two park-and-fly parking structures, surface parking lots, retail and commercial office buildings, restaurants, apartment complex developments, single family dwellings and the Sound Transit SeaTac/Airport Link Light Rail Station.

d. Will any structures be demolished?

No structures will be demolished as part of the non-project proposal.

e. What is the current zoning classification of the site?

The following zones exist within the City Center boundary. Zones are listed in order from commercial to residential zones:

- Community Business-Urban Center
- Office/Commercial Medium
- Neighborhood Business
- Urban High Residential Density
- Urban Medium Residential Density
- Urban Low Residential Density

Predominant zones include Community Business-Urban Center and Urban High Residential Density. Under the existing code and proposal, park-and-fly facilities are permitted outright in the Community Business-Urban Center and as a conditional use in the Office/Commercial Medium zones. See Figure 2.

f. What is the current Comprehensive Plan designation of the site?

The following Comprehensive Plan designations exist with the City Center boundary:

- Commercial High Density
- Commercial Medium Density
- Commercial Low Density
- Residential High Mixed Used
- Residential High Density
- Townhouse
- Residential Medium Density

The predominant Comprehensive Plan designation is Commercial High Density. See Figure 3.

In addition to the Comprehensive Plan land use designations, there are a number of goals and policies in the Comprehensive Plan and City Center Plan that are pertinent to this proposal. Please see Attachment 2 for a review of applicable Comprehensive Plan and City Center Plan goals and policies.

g. If applicable, what is the current Shoreline Master Program designation of the site?

This is not applicable to the City Center.

h. Has any part of the site been classified as an “environmentally sensitive” area? Specify.

- Bow Lake is located within the City Center boundary. It is a Class 1 Wetland and has an associated 100’ wetland buffer.
- Additionally, a steep slope in excess of 40% is located to the west of 32nd Avenue South between South 176th and South 180th Streets.

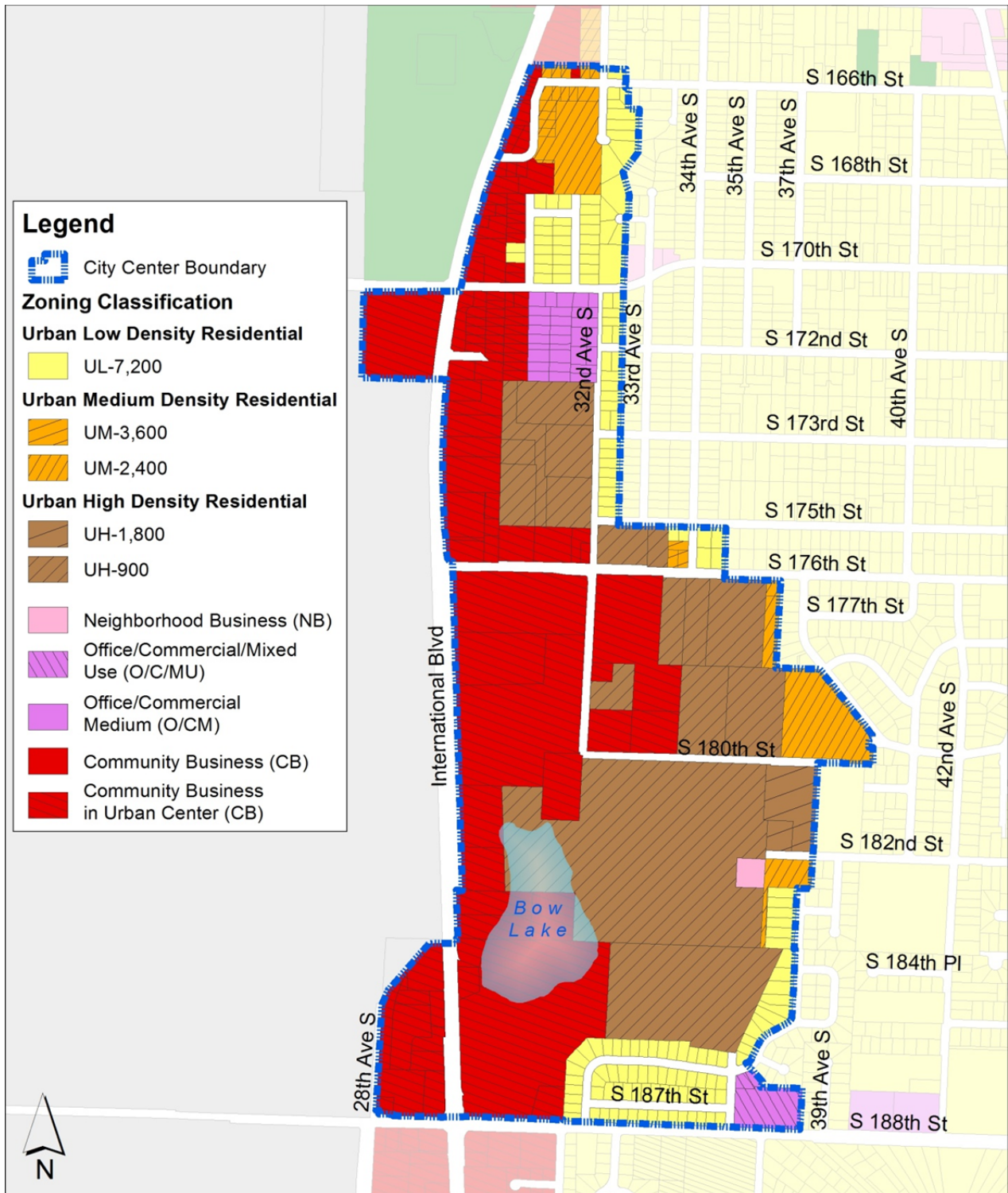


Figure 2 City Center Zoning Designations

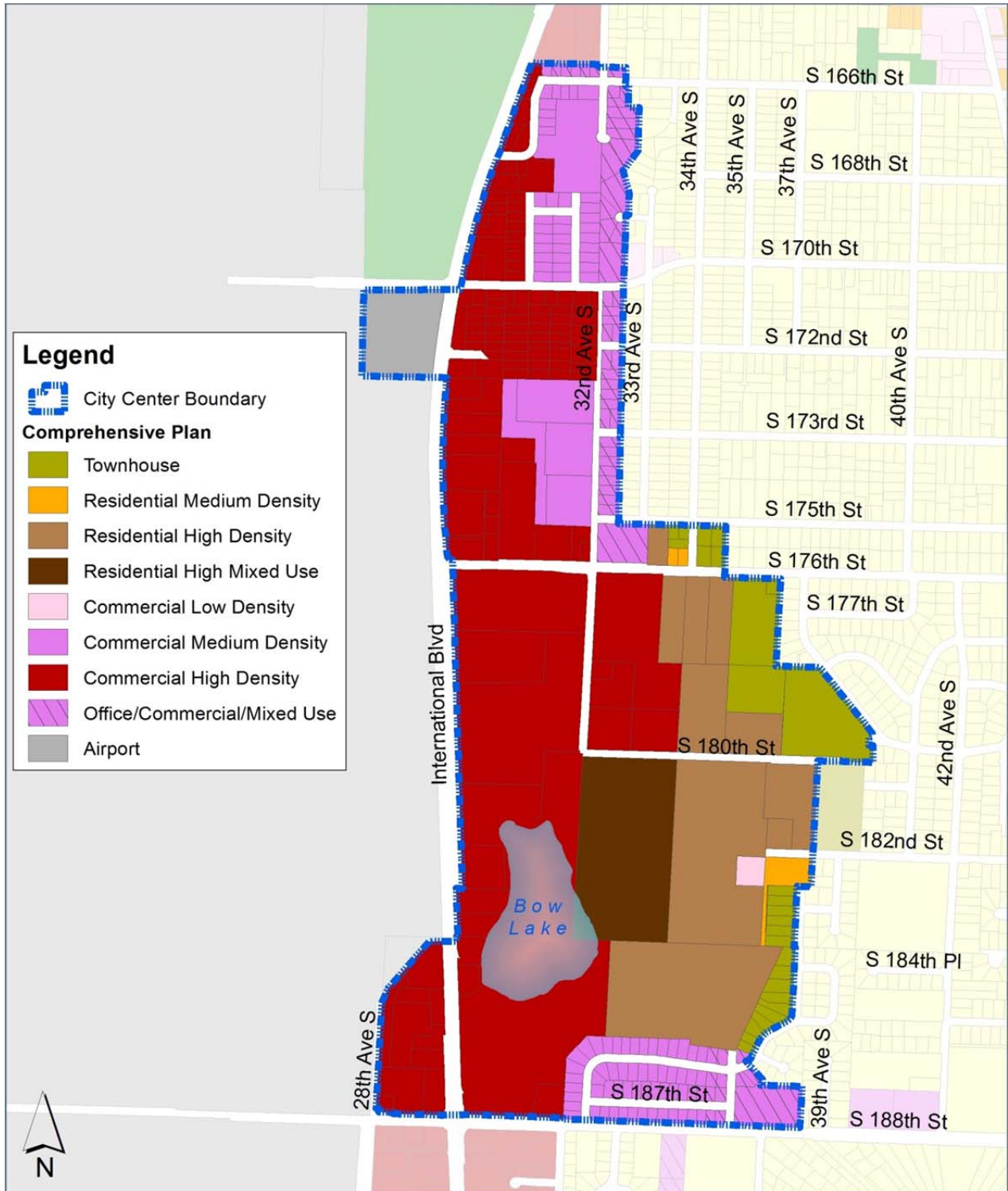


Figure 3 City Center Comprehensive Plan Land Use Designations

i. Approximately how many people would reside or work in the completed project?

As a non-project action, adoption of the proposal would not directly provide housing or employment.

At the project-specific development level, the number of people that could work at a potential park-and-fly parking structure would vary depending on the nature of the project. Factors could include the size of the project, if valet parking is available and the amount of commercial space required, among others.

j. Approximately how many people would the completed project displace?

This is a non-project action and it is not anticipated that people would be displaced as a result of this proposed code amendment. Under the proposed amendments, park-and-fly facilities would continue to be permitted only in the commercial zones and are unlikely to displace people.

k. Proposed measures to avoid or reduce displacement impacts.

As a non-project action, the proposal would not cause displacement. No mitigation is proposed.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any.

- The proposed code amendments replace the existing vehicular circulation standards that operate within the City Center with standards set forth in SMC Title 11, Streets, Sidewalks, and Public Thoroughfares. In order to maintain consistency with adopted goals and policies in the Land Use Element of the Comprehensive Plan and the City Center Plan, consider maintaining the existing City Center Vehicular Access Requirements, as described in SMC 15.35.210 Sections A through F.
- The proposed standards implement the principles identified in the City Center Subarea Plan in the following ways;
 - Promoting a mix of uses by increasing the minimum amount of ground floor commercial space to be built when park-and-fly projects are constructed;
 - Enhancing pedestrian-oriented design by addressing building scale, façade treatments, building materials and ground floor use requirements;
 - Concealing parking by increasing requirements for the screening of parking including design treatments for the first two stories of garages in order that they have the appearance of commercial buildings, the complete screening of parking at grade and screening treatments for parking in upper levels.
 - Contributing to distinctive buildings and dramatic skylines through encouraging enhanced building facades and architectural focal points, especially for buildings located at intersections.
 - Promoting the pedestrian environment by requiring the construction of significant on-site publicly accessible open space to activate building frontage while reducing the minimum amount of open space required per development site.

- No additional mitigation is proposed.

9. Housing

a. **Approximately how many units would be provided? Indicate whether high, middle, or low-income housing.**

While the proposed code amendments allow mixed use residential developments, it is unknown whether this type of development might be built in association with a park-and-fly project.

b. **Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.**

This non-project action is not expected to eliminate any housing units. Should a future proposal to construct a parking structure require the demolition of housing, this will be assessed at the time of an application.

c. **Proposed measures to reduce or control housing impacts.**

As a non-project action, the proposal would not result in housing impacts and no mitigation is proposed.

Should a future proposal to construct a parking structure require the demolition of housing, this will be assessed at the time of an application.

10. Aesthetics

a. **What is the tallest height of any proposed structure(s), not including antennas; what is/are the principal exterior building material(s) proposed?**

The tallest existing buildings in City Center include two 12-story office towers and a 14-story hotel.

There is currently no limit on the maximum height of any building permitted within the Community Business-Urban Center (CB-C) zone, the predominant commercial zone within the City Center, except for the limitations imposed by Federal Aviation Agency (FAA) regulation.

For properties located along International Boulevard, and assuming an average elevation of 350 feet above sea level, the maximum building height permitted in accordance with FAA standards is estimated to be approximately 225 feet which could equate to around 22 floors. An elevation of 400 feet above sea level could allow a maximum building height of 150 feet or around 15 floors. These estimated building heights do not take into consideration other site constraints.

The proposed code amendments would not change any existing height limits in City Center.

b. **What views in the immediate vicinity would be altered or obstructed?**

The proposal is a non-project action that will not directly alter or obstruct views. Within the Community Business-Urban Center (CB-C) zone, the predominant commercial zone within the City Center and the only zone where park-and-fly structures are allowed outright, the existing standard for all commercial buildings allows a maximum 75% lot

coverage and heights are limited only by FAA regulations. The proposal would not change these requirements.

While the 12-acre water body called Bow Lake is located within the City Center, it is situated within a depression, and there are no views to the lake other than from upper stories of buildings that immediately surround it.

There are currently no designated view corridors within the City Center boundary and scenic vistas are already integrated with views of existing development including multi-story hotel and office towers, the Seattle-Tacoma International Airport and various billboards and other advertising mechanisms.

c. Proposed measures to reduce or control aesthetic impacts.

- The proposed code amendments include a number of revised design standards with the intent of promoting buildings that are visually interesting, enhancing the visual character of new and redeveloped park-and-fly projects, and contributing to the overall character of the City Center. Proposed standards include:
 - Requiring one story buildings to have the appearance of two story buildings to enhance the street wall;
 - Increasing requirements for vertical and horizontal articulation and the screening of parking at grade and upper levels to reduce the bulk and scale of projects;
 - Requiring the first two stories of buildings fronting International Boulevard to have the appearance of a commercial building to create a more human scale of development;
 - The addition of design treatments within the first two stories of structures and requirements for high quality building materials;
 - The prohibition of building materials that might detract from the pedestrian-oriented environment such as reflective materials, corrugated fiberglass and chain link fencing; and,
 - Requiring architectural focal points along the front façade and architectural expression on the corners of buildings.
- No additional mitigating measures are needed or proposed.

11. Light and Glare

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

The proposal is a non-project action that would not directly produce light and glare. The proposed amendments are not expected to result in a change in the amount of light or glare produced or the timing of light and glare from park and fly facilities.

Future park-and-fly developments would create light and glare primarily from vehicular traffic and building lighting. It is anticipated that lighting would be typical of that found in a highly urban commercial area such as the City Center. Any future park-and-fly structure built within the City Center will be required to meet the minimum safety

lighting standards and provisions that minimize light and glare impacts as specified within SMC Title 17 Crime Prevention through Environmental Design (CPTED).

b. Could light or glare from the finished project be a safety hazard or interfere with views.

See response to Question 11.a, above.

c. What existing off-site sources of light or glare may affect your proposal?

Light and glare in City Center are typical of a highly developed urban area and include street lights, building lighting and lighted signs. It is unlikely that the proposed code amendments would result in an increased sensitivity to light and glare for park-and-fly facilities. However, potential impacts associated with a specific development proposal will be assessed at the time of a project specific SEPA review.

d. Proposed measure to reduce or control light and glare impacts, if any.

- As a non-project action, the proposal would not result in light and glare impacts. No mitigation is proposed.
- Any future park-and-fly development built within the City Center will be required to meet the minimum safety lighting standards and provisions that minimize light and glare impacts as specified within SMC Title 17 Crime Prevention through Environmental Design (CPTED).

12. Recreation

a. What designated and informal recreational opportunities are in the immediate vicinity?

Currently the City Center does not have any areas designated for recreation use. Informal recreation is limited to walking along sidewalk routes located within the right of way.

b. Would the proposed project displace any existing recreational uses? Describe.

As a non-project action, the proposal would not displace any existing recreational uses. There are no known recreational uses in the City Center that could be displaced by a potential future development.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant.

As a non-project action, the proposal would not result in impacts on recreational opportunities. No mitigation is proposed.

Recreational centers and health clubs are a permitted uses within most zones in the City Center and could occupy the commercial space in a park-and-fly development. Additionally, should a park-and-fly development include multi-family residential uses, recreational amenities would be required per the City's Design Standards for Multi-Family Housing (SMC Chapter 15.19).

13. Historic and Cultural Preservation

a. Are there any places or objects listed on, or proposed for, national, state, or local preservation registers known to be on or next to the site? Generally describe.

A review of the Washington State Department of Archeology and Historic Preservation and National Register of Historic Places does not list any places of historic significance within the City Center. There are no known sites proposed to be included on a national, state or local preservation list.

b. Generally describe any landmarks or evidence of historic, archaeological, scientific, or cultural importance known to be on or next to the site.

The Seattle-Tacoma International Airport is located immediately to the west of the City Center, abutting its western boundary.

c. Proposed measures to reduce or control impacts.

Because this is a non-project action that would not impact historic or cultural resources and that there are no known places of historic or cultural significance within the City Center Boundary, no adverse impacts are anticipated. No mitigation is proposed.

14. Transportation

a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans.

Public streets and highways that serve the City Center include the following (see also Figures 2 or 3 for street locations):

- SR-99/International Boulevard/Pacific Highway South, the principal subregional arterial serving the city provides the primary north-south access in the City Center. International Boulevard is a five lane urban arterial with a posted speed of 45 miles per hour. There is a raised median that opens for turn pockets at major intersections and select driveways. According to the City Center Plan Draft Supplemental Environmental Impact Statement (SEIS), approximately 75 percent of all City Center traffic uses International Boulevard.
- Other arterials that serve the City Center include South 188th Street, a primary east-west arterial that interconnects with both I-5 and the freeway terminus of SR-509; South 176th Street, a minor urban arterial leading east from the City of SeaTac to Tukwila; and South 170th Street, a collector arterial leading into a residential area and connecting with Military Road to the east.
- Various north-south and east-west non-arterials also serve the City Center.

b. Is the site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?

The City Center is served by multiple public transit options, including light rail and regional express buses. More specifically, the SeaTac/Airport Link Light Rail Station is located at a central location within the City Center, and King County Metro RapidRide and other buses run along International Boulevard within the area.

c. How many parking spaces would the completed project have? How many would the project eliminate?

As a non-project action, the proposal would not directly provide parking spaces. However, the proposal does set standards that will guide future development of park-and-

fly facilities. As proposed, future park-and-fly development would be required to adhere to the following parking standards:

- Commercial/Office/Residential uses would continue to be required to follow the minimum and maximum parking provisions identified in SMC 15.35.800. The proposal would not change this standard.
- New structured park-and-fly parking developments would be required to follow the parking stall maximum established for park-and-fly stalls as designated within the proposed Section 15.35.1013, which revises the maximum number of base parking stalls from a total of 300 base stalls to a maximum of 1,000 base stalls. Additional parking stalls could be acquired through the revised Parking Bonus Incentive Program identified in Section 15.35.1020 of the proposed code.

It should be noted that the Zoning Code Ad Hoc Committee, with representation of park-and-fly developers, has advised that it is unlikely that a parking structure of more than seven stories would be economically feasible.

For additional reference, the two existing park-and-fly developments constructed within the City Center, which were both developed with development agreements rather than following the existing Parking Bonus Incentive Program, are seven stories or below and have a total stall count of 1,694 for the WallyPark garage, and 1,039 for the MasterPark garage (See Attachment 3 for additional information).

d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? Generally describe (indicate whether public or private).

The proposed code amendment does not specifically require any new road connections. However, as part of the current and proposed Parking Bonus Incentive Program, additional parking stalls are granted to developments that elect to provide additional dedicated right of way in accordance with the City Center Vehicular and Pedestrian Access Plan.

The proposed code amendments also replace the existing vehicular circulation standards that operate within the City Center with standards set forth in SMC Title 11, Streets, Sidewalks, and Public Thoroughfares. This change would remove the existing requirements tailored to meet City Center transportation goals and policies, including the design of the street system to create blocks which are no greater than 400 feet on a side.

In 2010, the Transpo Group conducted a Traffic Analysis Update for a portion of the City Center in which they concluded that the interconnected system of local streets proposed in the City Center Plan would be desirable to provide access and circulation to future development and to reduce the potential impacts on adjacent arterials. Please see also the discussion of Comprehensive Plan and City Center Plan goals and policies in Attachment 2.

e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? Generally describe.

The City Center Boundary adjoins the Seattle-Tacoma International Airport, and contains the SeaTac/Airport Link Light Rail Station. It is anticipated that future park-and-fly facilities will serve passenger traffic to the airport.

f. How many vehicular trips per day would be generated by the completed project? Indicate when the peak volumes would occur.

The non-project proposal will not generate vehicular trips.

Development under the proposed code amendments, however, will provide parking facilities for air passengers at the Seattle-Tacoma airport. Because the airport is an ongoing existing use, it is likely that some of the future users at a new facility are currently driving to other parking facilities or drop-off locations at or near the airport.¹ In the short term, the primary traffic impact associated with new park-and-fly facilities may be diversion of trips from other routes within and around the City Center area, rather than creation of new trips. In the long term, growth in air passenger traffic would be associated with new trips to park-and-fly facilities.

The development of new facilities may also encourage an unknown number of air passengers to drive and park at a new facility, depending on individual perceptions of cost and convenience of park-and-fly facilities compared to other options.

Future use of park-and-fly facilities could be expected to fluctuate with changes in passenger activity at the airport. In typical years, passenger activity reaches peak levels in July and August, when over 3 million passengers per month pass through the airport. Passenger activity exceeds 2.2 million passengers in every month except January, February and November.²

For the purpose of highlighting the different sizes of park-and-fly garages potentially allowed within the City Center, Attachment 3 Existing Development Examples compares two existing park-and-fly developments in the City Center, specifically the WallyPark and MasterPark garages, to potential new park-and-fly developments that might be allowed utilizing the current and proposed park-and-fly development standards. A summary of this comparison follows:

- *WallyPark Development:* The WallyPark development was built through a development agreement, and did not use the current development standards. As constructed, the WallyPark development comprises a total of 1,694 park-and-fly stalls. If the same development were constructed in accordance with the proposed park-and-fly standards and the bonus incentive program were applied, the total

¹ In the environmental review for the Seattle-Tacoma International Airport Comprehensive Development Plan, the Surface Transportation Discipline Report (HNTB, 2007) states that trip generation associated with parking garages is based on the specific activity that the garage supported (such as air passengers, employees, air cargo, etc.). Similarly the City Center Plan notes that the future park and fly market will be driven by growth in air passengers.

² Environmental Review NEPA Environmental Assessment POS SEPA No. 07-09. Final Seattle-Tacoma International Airport Comprehensive Development Plan. Appendix H Surface Transportation Discipline Report. HNTB. July 2007.

number of park-and-fly stalls permitted would increase to 1,806 stalls. This is a total increase of 112 park-and-fly stalls.

- *MasterPark Development:* The MasterPark development was built through a development agreement, and did not use the current development standards. As constructed, the MasterPark development comprises a total of 1,039 park-and-fly stalls. If the same development were constructed in accordance with the proposed park-and-fly standards and the bonus incentive program were applied, the total number of park-and-fly stalls permitted would increase to 1,516 stalls. This is a total increase of 477 park-and-fly stalls.

Because the total number of stalls allowed through the proposed park-and-fly standards and bonus incentive program was not significantly higher than the number of stalls already allowed within the WallyPark development, the increase is not considered to have a significant adverse impact on the demands for transportation.

Any potential future development would be required to undertake a traffic impact analysis study which would identify vehicle trips associated with the proposal, including the number of peak hour trips generated inbound and outbound of a site. It is anticipated that, unlike office development, the new park-and-fly projects would not result in significant traffic at peak hours. Rather, park-and-fly garage traffic would be spread out throughout the day and week.

According to general traffic generation estimates and other analyses conducted as part of the City Center Draft Supplemental Environmental Impact Statement (DSEIS), the following conclusions were made:

In 1999, existing development in the City Center was estimated to generate 79,100 vehicle trips per average weekday (AWDT), and 7,470 trips during the afternoon peak hour between 4 and 6 pm.

By 2010 or beyond, the 79,100 vehicle trips per day were estimated to decrease to 69,600 as a result of the transit/HOV enhancements and TDM strategies. (Please note that the following transit/HOV enhancements have been made since 1999 including: the construction of the SeaTac-Airport Link Light Rail Station, which opened in 2009, King County Metro RapidRide express buses, which began service in 2010, and the implementation of Transportation Demand Management (TDM) strategies such as the City of SeaTac's Commute Trip Reduction program.)

g. Proposed measures to reduce or control transportation impacts.

- In order to provide access and circulation to future development and to reduce the potential impacts on adjacent arterials and to maintain consistency with the adopted Comprehensive Plan and City Center Plan, consider maintaining the existing City Center Vehicular Access Requirements, as described in SMC 15.35.210 Sections A through E.
- Future project-specific development proposals will be reviewed pursuant to SEPA and applicable provisions of the City of SeaTac Municipal Code. This will include preparation of a traffic impact analysis study which would identify vehicle trips

associated with the proposal, including the number of peak hour trips generated inbound and outbound of a site.

15. Public Services

a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? Generally describe.

The non-project proposal would not directly result in an increased need for public services.

The SeaTac Fire Department is responsible for delivering fire protection and emergency medical services to the City. The SeaTac Police Department provides law enforcement to the City. The City Center area is served by the Highline School District.

The City does not anticipate any additional need for public services.

b. Proposed measures to reduce or control direct impacts on public services.

- As a non-project action, the proposal would not result in public service impacts. No mitigation is proposed.
- Any future park-and-fly development will be required to meet the standards specified in SMC Chapter 17 Crime Prevention Through Environmental Design, which promotes an emphasis on effective and functional design of the built environment to create a safer environment and lower incidences of crime.

16. Utilities

a. Circle utilities currently available at the site:

Electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other _____.

The City Center is a highly urbanized area and is served by electricity, natural gas, water, refuse service, telephone, sanitary sewer, and septic systems.

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

- As a non-project action, no utilities are proposed as part of the proposal.
- Any utilities required to service a future parking structure development will be assessed as part of an application for a building permit.

C. SIGNATURE

The above answers are true and complete to the best of my knowledge, I understand that the lead agency is relying on them to make its decision.

Signature: 
Deborah Munkberg, AICP

Date Submitted: August 2, 2012

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

(Do Not Use This Sheet For Project Actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment. When answering these questions, be aware of the extent of the proposal, or how the types of activities likely to result from the proposal would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

-
- a. How would the proposal be likely to increase discharge to water, emissions to air, production, storage, or release of toxic or hazardous substances, or production of noise?**

The proposed amendments are not likely to result in significant increases in discharge to water, air or produce toxic/hazardous substance.

While the construction of new park-and-fly garages could result in increased traffic within the City Center, and a corresponding increase in emissions to air related to vehicular traffic, any new projects would be within the range of uses and densities envisioned for the area, as evaluated in the City of SeaTac's Comprehensive Plan Draft and Final Environmental Impact Statements, in the City Center Plan's Draft and Final Environmental Impact Statements, and in the Determination of Non-Significance issued for the adoption of the SMC 15.35 Special Standards for the City Center.

Please see also Sections B.2, B.3, and B.7 of this Environmental Checklist.

Proposed measures to avoid or reduce such increases are:

No measures are proposed or required. Impacts related to specific development proposals will be mitigated through application of the City's development regulations, and subject to appropriate environmental review, as needed.

- b. How would the proposal be likely to affect plants, animals, fish, or marine life?**

The proposed amendments would not likely increase impacts to plants, animals, fish, or marine life. Please see discussion under Sections B.4 and B.6 of this Environmental Checklist.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

No measures are proposed or required. Impacts related to specific developments at the project level will be mitigated subject to appropriate environmental review, as needed.

c. How would the proposal be likely to deplete energy or natural resources?

The proposed amendments would not likely deplete energy or natural resources. Please see Section B.6 of this Environmental Checklist.

Proposed measures to protect or conserve energy and natural resources are:

No measures are proposed or required. Impacts related to specific development proposals will be mitigated through application of the City's development regulations, and subject to appropriate environmental review, as needed.

d. How would the proposal be likely to use or affect environmentally sensitive areas or areas designed (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplain, or prime farmlands?

The proposed amendments would not likely use or affect environmentally sensitive areas or areas designed for governmental protection. Please see discussion under Sections B.4, B.5, B.8, B.12 and B.13 of this Environmental Checklist.

Proposed measures to protect such resources or to avoid or reduce impacts are:

No measures are proposed or required. At the project level, the City would protect such resources through the continued application of SEPA and the adopted Environmentally Sensitive Areas Chapter of the SeaTac Municipal Code (Title 15). Existing Environmental Management Policies (8.3E through 8.3H) in the City's Comprehensive Plan address wildlife habitat protection.

e. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

The proposed amendments are consistent with the land use and zoning designations contained in the City's Zoning Code or Comprehensive Plan.

With regard to adopted goals and policies in the Comprehensive Plan and City Center Plan, existing regulations in SMC 15.35.210 are intended to implement the intent of a reduced block size through the addition of new streets as development occurs and to provide additional street connections. The regulations establish right-of-way standards, a standard of block faces that are no more than 400 feet in length on a side, ground level retail requirements and public art requirements.

The proposal would eliminate these requirements and replace them with SMC Title 11, Streets, Sidewalks and Public Thoroughfares. Because SMC Title 11

does not promote increased connectivity in City Center, this change is not consistent with policies in the Comprehensive Plan and City Center Plan that speak to this intent. Please see Attachment 2 for a complete discussion of adopted goals and policies.

The proposed amendments would not affect shoreline uses. No parcels within the City Center are located in the vicinity of areas subject to the City's Shoreline Master Program.

Please see Section B.8 of this Environmental Checklist.

Proposed measures to avoid or reduce shoreline and land use impacts are:

- In order to provide access and circulation to future development and to reduce the potential impacts on adjacent arterials and to maintain consistency with adopted Comprehensive Plan and City Center Plan, consider maintaining the existing City Center Vehicular Access Requirements, as described in SMC 15.35.210, Sections A through F.

f. How would the proposal be likely to increase demands on transportation or public services and utilities?

Currently, park-and-fly garages are allowed within the City Center. Because the proposed changes do not alter the maximum height or density requirements for park-and-fly or commercial developments currently allowed in the City Center, it is not anticipated that these amendments would lead to significant impacts on demand for transportation facilities, public services or utilities in the area. Furthermore, it is anticipated that the proposed code amendments would result in park-and-fly projects within the range of uses and densities envisioned for the area in the SeaTac Comprehensive Plan and City Center Subarea Plan, and as evaluated in the City of SeaTac's Comprehensive Plan Draft and Final Environmental Impact Statements, in the City Center Plan's Draft and Final Environmental Impact Statements, and in the Determination of Non-Significance issued for the adoption of the SMC 15.35 Special Standards for the City Center.

While the construction of new park-and-fly garages could result in some increased traffic within the City Center, and a corresponding increase in emissions to air related to vehicular traffic, because park-and-fly traffic is spread out throughout the day, new park-and-fly garages would likely have less impact on peak traffic hours than office developments of a similar size that are currently allowed within the area.

For the purpose of highlighting the different sizes of park-and-fly garages potentially allowed within the City Center, a comparison of the two existing park-and-fly developments in the City Center, specifically the WallyPark and MasterPark garages, was conducted. A summary of this comparison follows (see also Attachment 3):

- *WallyPark Development:* The WallyPark development was built through a development agreement, and did not use the current development standards. As constructed, the WallyPark development comprises a total of 1,694 park-

and-fly stalls. If the same development were constructed in accordance with the proposed park-and-fly standards and the bonus incentive program were applied, the total number of park-and-fly stalls permitted would increase to 1,806 stalls. This is a total increase of 112 park-and-fly stalls.

- *MasterPark Development:* The MasterPark development was built through a development agreement, and did not use the current development standards. As constructed, the MasterPark development comprises a total of 1,039 park-and-fly stalls. If the same development were constructed in accordance with the proposed park-and-fly standards and the bonus incentive program were applied, the total number of park-and-fly stalls permitted would increase to 1,516 stalls. This is a total increase of 477 park-and-fly stalls.

Because the total number of stalls allowed through the proposed park-and-fly standards and bonus incentive program was not significantly higher than the number of stalls already allowed within the WallyPark development, the increase is not considered to have a significant adverse impact on the demands for transportation.

It should be noted that there are approximately six potential sites where park-and-fly garage developments would likely be constructed within the City Center. If all five of these sites were to be redeveloped as park-and-fly garages, under the proposed standards, the resulting developments would be consistent in scale with the two existing park-and-fly garage developments currently located within the City Center. Because park-and-fly traffic is spread out throughout the day, new park-and-fly garages would likely have less impact on peak traffic hours than office developments of a similar size that are currently allowed within the area. Additionally, new park-and-fly projects built under the proposed standards would have increased building façade and ground floor commercial requirements which are consistent with the building quality, density and uses envisioned within both the SeaTac Comprehensive Plan and City Center Subarea Plan.

Please see also Sections B. 14, B.15 and B.16 of this Environmental Checklist.

Proposed measures to reduce or respond to such demand(s) are:

- No measures are required or proposed. Potential impacts related to specific developments at the project level will be mitigated subject to appropriate environmental review, as needed.

g. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

The proposed amendments are not in conflict with local, state, or federal laws and are compatible with environmental protection requirements.

- The proposed code has been subject to a 60 day review of the Washington State Department of Community, Trade and Economic Development.
- The proposed code amendment will be required to satisfy all necessary government review and approval process, as required by Revised Code of Washington and Washington Administrative Code.

Attachment 1. Proposed Code Amendments

The proposed code amendments to the City Center Park-and-Fly Parking standards can be found on the following pages.



Proposed Code Amendments for City Center Park-and-Fly Parking Developments

**List of All Proposed Code Amendments for the
City Center Park-and-Fly Parking Standards Update
July 26, 2012**

SMC TITLE 15 ZONING CODE

Chapter 15.10 Land Use Definitions

Revised Sections

- 15.10.446 Parking, Commercial (Non-Park-and-Fly)
- 15.10.175.03 Development Site-Stand-Alone Parking Structures
- 15.010.175.03a Figure of Development Site-Stand-Alone Parking Structures

Deleted Sections

- 15.10.449 Parking Structure, Stand-Alone

New Sections

- 15.10.447 Parking, Commercial Park-and-Fly

Chapter 15.12 Zone Classification Use Charts

Revised Sections

- 15.12.050 Government/Office, Business Uses

Chapter 15.14. Development Standards-Tree Retention and Landscaping

Revised Sections

- 15.14.020 Authority and Applications
- 15.14.060 Landscaping Standards for Government/Office, Business Uses

Chapter 15.15 Development Standards-Parking and Circulation

Revised Sections

- 15.15.030 Parking Space Requirements for Government/Office, Business Uses
- 15.15.130 Off-Site Parking Location

Chapter 15.35 Special Standards for the City Center

Revised Sections

- 15.35.140 Government/Office, Business Uses
- 15.35.810 Maximum Parking Requirements
- 15.35.820 Surface Parking
- 15.35.900 Non-Park-and-Fly Parking Structures
- 15.35.949* Commercial (Non-Park-and-Fly) Parking Structures (**Note: Was 15.35.905 Stand-Alone Parking Structures*)
- 15.35.950 Parking Bonus Incentive Program for Commercial (Non-Park-and-Fly) Parking Structures

New Sections

- 15.35.901 Authority and Application
- 15.35.1000-15.35.1053*Park-and-Fly Parking Developments (**Note: These sections contain the largest number of code changes related to development standards and the bonus program for park-and-fly commercial parking.*)

**SMC TITLE 17 CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN
(CPTED)**

Chapter 17.08 Purpose, Principles and Application

Revised Sections

17.28.020 Application

Chapter 17.28 Parking Structures

Revised Sections

17.28.010 Application

17.28.030 Parking Structure Standards

Chapter 15.10 Land Use Definitions

Revised sections include: 15.10.446, 15.10.175.03, 15.10.175.03a

Deleted sections include: 15.10.449

New sections include: 15.10.447

15.10.446 Parking, Commercial (Non-Park-and-Fly) Lot, Public/Private

A use of land or structure for the parking of motor vehicles as a commercial enterprise for which fees are charged, except for airport parking for a fee. Commercial (Non-Park-and-Fly) Parking does not include Commercial Park-and-Fly Parking.

~~An area used exclusively for the parking of motor vehicles for a fee for any period of time.~~

15.10.447 Parking, Commercial Park-and-Fly

A use of land or structure for the parking of motor vehicles for airport parking for a fee for any period of time. Commercial Park-and-Fly Parking does not include Commercial (Non-Park-and-Fly) Parking.

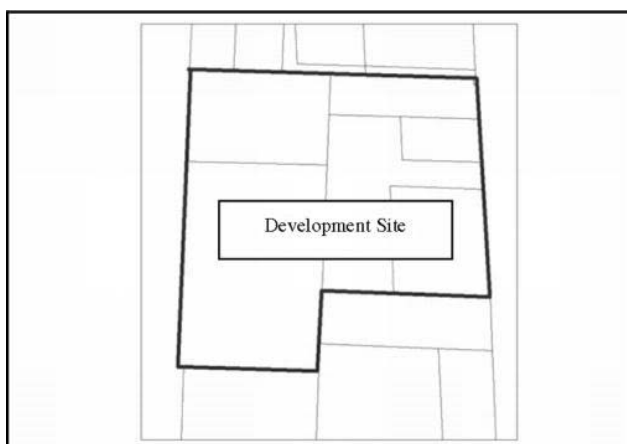
15.10.449 Parking Structure, Stand-Alone

~~A parking structure used exclusively for the parking of motor vehicles, either public or private, for a fee, for any period of time.~~

15.10.175.03 Development Site ~~—Stand-Alone Parking Structures~~

A development site is the sum total of all parcels of property incorporated into ~~the a~~ development at any point of time. This includes the incorporation of any additional properties into the development site. (See Figure 15.10.175.03a) (Ord. 05-1002 § 3)

Figure 15.10.175.03a. DEVELOPMENT SITE ~~—STAND-ALONE~~
~~PARKING STRUCTURES~~



Chapter 15.12 Zone Classification Use Charts

(Revised sections include: 15.12.050)

15.12.050 Government/Office, Business Uses

ZONES: UH – Urban High Density O/CM – Office/Commercial Medium

P – Park NB – Neighborhood Business BP – Business Park

MHP – Mobile Home Park CB – Community Business O/C/MU – Office/Commercial/Mixed Use

UL – Urban Low Density ABC – Aviation Business Center T – Townhouse

UM – Urban Medium Density I – Industrial/Manufacturing

P – Permitted Use; C – Conditional Use Permit

USE #	LAND USE	ZONES												
		P	MHP	UL	UM	UH	NB	CB	ABC	I	O/CM	BP	O/C/MU	T
GOVERNMENT/OFFICE USES														
071	Social Service Office					C*	P	P*	P*	P	P*	C*(1)	P*	
072	Public Agency Office					P*	P	P*	P*	P	P*	C*(1)	P*	
073	Public Agency Yard	C(2)		P(4)				P*	C*	P	C*	C*	C*	
074	Public Archives	C(3)					C	P*	P*	P	P*	C*	P*	
075	Court							P*	P*	P	P*	C*(1)	P*	
076	Police Facility	P		C	P	P*	P	P*	P*	P	P*	P*	P*	
077	Fire Facility	P		C	P	P*	P	P*	P*	P	P*	P*	P*	
079	Helipad/Airport and Facilities									P				
080	Utility Use			C	C	C*	C	C*	P*	P	C*	C*	C*	
081	Utility Substation			C	C	C*	C	P*	P*	P	C*	C*	C*	
082	Financial Institution						P	P*	P*	P	P*	C*(1)	P*	
083	City Hall			P	C*	C*		P*	P*		P*	C*	P*	
083.5	Secure Community Transition Facility							C*(5)	C*(5)	C(5)	C*(5)	C*(5)		
BUSINESS SERVICES USES														
084	Landscaping Business							P*	P*	P		P*		
085	Butterfly/Moth Breeding						P	P*	P*	P				
086	Construction/Trade							C*	P*(1)	P	C*			
087	Truck Terminal							C*	P*(1)	P	C*			

088	Airport Support Facility								P*					
089	Warehouse/Storage						C	C*	P*	P	C*	P*		
090	Professional Office						P*	P	P*	P*	P	P*	P*(1)	P*
091	Heavy Equipment Rental								C*	P				
092	Misc. Equipment Rental Facility						C	P*		P	P*(1)			
093	Auto Rental/Sales							P*	P*(1)	P	C*(1)			
094	<u>Commercial (Non-Park-and-Fly) Parking</u> Public/Private Parking						C	P*	P*	P	C*(1)			
<u>094.5</u>	<u>Commercial Park-and-Fly Parking</u>						<u>C</u>	<u>P*</u>	<u>P*</u>	<u>P</u>	<u>C*(1)</u>			
095	Large Vehicle Repair									P				
096	Heavy Equipment Repair									P				
097	R and D/Testing						C	C*	P*	P	C*	P*		
098	Commercial/Industrial Accessory Uses						P	P*	P*	P	C*			

* See Chapters [15.13](#) and [15.35](#) SMC for additional development standards.

- (1) Accessory to primary use not to exceed twenty percent (20%) of primary use.
- (2) A public agency yard located on property within the park zone may be used as a combined maintenance facility for park and nonpark purposes; provided, that the facility shall be no more expansive than that which is reasonably expected to be needed for park maintenance when park facilities are fully developed.
- (3) A public archives facility located on property within the park zone is limited to existing structures.
- (4) Applies only to City of SeaTac Public Works Maintenance Facility located at the Glacier High School site, on an interim basis. The City of SeaTac shall be allowed to expand the maintenance facility at that site to the extent authorized by the City Council; until such time as a replacement facility at another site is operational.
- (5) Secure community transition facilities are subject to the CUP-EPF siting process (SMC 15.22.035).

Chapter 15.14 Development Standards – Tree Retention and Landscaping

(Revised sections include: 15.14.020 and 15.14.060)

15.14.020 Authority and Application

- A. The provisions of this chapter shall apply to:
1. All new developments on vacant land requiring building permits; or
 2. When the gross floor area (gfa) of a building/complex expands beyond twenty percent (20%) of the total existing gfa, the current landscape standards shall be applicable and integrated into the redevelopment. Within the Neighborhood Business (NB) zone, the provisions of this chapter shall apply when the complex expands beyond forty percent (40%) of the total existing gfa; or
 3. Upon the change in use of any property to a commercial (non-park-and-fly) parking or commercial park-and-fly use ~~public/private parking lot~~; or
 4. Upon the conversion of any outdoor space of two hundred (200) square feet or greater to a business use or parking, the current landscape standards shall be integrated into that portion of the site to the greatest extent feasible.
- B. The following uses are exempt from the provisions of this chapter.
1. Single-family dwellings;
 2. Residential accessory uses; and
 3. Subdivisions (except as provided under SMC 15.14.110) and short subdivisions in regard to perimeter and street landscape proportions only.
- C. Where the width of a required landscape strip exceeds the normally required setback of a zone or specific use, the required setback shall be increased to accommodate the full width of the required landscaping, with the following exception:

The street frontage landscape strip requirement shall not apply to uses in the urban high-urban center residential (UH-UCR) zoning category, community business zoning category in the urban center (CB-C), or office/commercial medium (O/CM) zoning category, and within the area delineated as the City Center. Within the City Center, front yard open space as per SMC 15.35.420 shall be required in lieu of street frontage landscaping.

If the normal required landscaping is reduced through this exception for all applicable zones except in the City Center, fifty percent (50%) of said landscaping shall be placed into plazas, roof-top gardens and other pedestrian amenities, and street trees shall be planted within the public right-of-way in locations and amounts to be determined by the City Manager or designee.

- D. When an existing building precludes installation of the total width of required landscaping, the landscaping shall be installed to the extent possible and the remaining required landscaping shall be installed elsewhere on the site to provide the best possible screening.

- E. Other Standards Applicable. Except as specified in this section of the Zoning Code, all other relevant standards and requirements in this code shall apply. (Ord. 04-1010 § 8; Ord. 00-1037 § 3; Ord. 98-1019 § 13; Ord. 95- 1028 § 10; Ord. 95-1016 § 20; Ord. 92-1041 § 1)

15.14.060 Landscaping Standards for Government/Office, Business Uses

USE #	LAND USE	STREET FRONTAGE (Type/Width)	BUILDING FACADE IF > 30 FT. HIGH OR > 50 FT. WIDE (Type/Width)	SIDE/REAR YARDS (Type/Width)	SIDE/REAR BUFFER FOR NON-COMPATIBLE ZONES (Type/Width)	PARKING LOT LANDSCAPE STANDARDS APPLICABLE*
GOVERNMENT/OFFICE						
071	Social Service Office	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
072	Public Agency Office	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
073	Public Agency Yard	III/20 ft.	IV/5 ft.	III/5 ft.	II/20 ft. (SF)	Yes
074	Public Archives	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
075	Court	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
076	Police Facility	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
077	Fire Facility	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes
079	Helipad/Airport Facility	I/10 ft.	–	I/10 ft.	I/20 ft. (RES)	–
080	Utility Use	III/10 ft.	IV/5 ft.	IV/10 ft.	II/10 ft. (SF)	Yes
081	Utility Substation	I/10 ft.	–	I/10 ft.	–	–
082	Financial Institution	IV/10 ft.	IV/5 ft.	IV/5 ft.	II/10 ft. (SF)	Yes
083	City Hall	IV/10 ft.	IV/5 ft.	III/10 ft.	I/20 ft. (RES)	Yes
083.5	Secure Community Transition Facility**	I/10 ft.	IV/5 ft.	I/10 ft.	I/20 ft. (RES)	Yes
BUSINESS SERVICES						
084	Landscaping Business	II/10 ft.	IV/5 ft.	II/10 ft.	I/20 ft. (RES)	Yes
085	Butterfly/Moth Breeding	III/10 ft.	IV/5 ft.	III/5 ft.	I/10 ft. (RES)	Yes
086	Construction/Trade	III/10 ft.	IV/5 ft.	II/5 ft.	I/10 ft. (RES)	–
087	Truck Terminal	II/10 ft.	IV/5 ft.	II/5 ft.	I/20 ft. (RES)	Yes
088	Airport Support Facility	IV/10 ft.	IV/5 ft.	II/5 ft.	I/20 ft. (RES)	Yes
089	Warehouse/Storage	II/10 ft.	IV/5 ft.	II/5 ft.	I/20 ft. (RES)	Yes
090	Professional Office	IV/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (RES)	Yes
091	Heavy Equipment Rental	III/10 ft.	IV/5 ft.	III/5 ft.	I/10 ft. (RES)	Yes

092	Misc. Equipment Rental Facility	IV/10 ft.	IV/5 ft.	II/5 ft.	I/10 ft. (SF)	Yes
093	Auto Rental/Sales	IV/10 ft.	IV/5 ft.	II/10 ft.	I/20 ft. (RES)	Yes
094	<u>Commercial (Non-Park-and-Fly) Parking</u> Public/Private Parking	III/10 ft.	IV/5 ft.	II/10 ft.	II/20 ft. (RES)	Yes
094.5	<u>Commercial Park-and-Fly Parking</u>	<u>III/10 ft.</u>	<u>IV/5 ft.</u>	<u>II/10 ft.</u>	<u>II/20 ft. (RES)</u>	<u>Yes</u>
095	Motor Freight Repair	II/10 ft.	IV/5 ft.	II/10 ft.	I/20 ft. (RES)	Yes
096	Heavy Equipment Repair	II/10 ft.	IV/5 ft.	II/5 ft.	II/20 ft. (RES)	Yes
097	R and D/Testing	III/10 ft.	IV/5 ft.	II/10 ft.	I/20 ft. (RES)	Yes
098	Commercial/Industrial Accessory Uses	II/10 ft.	IV/5 ft.	III/5 ft.	II/10 ft. (SF)	Yes

*See SMC [15.14.090](#).

**Requirements listed here are the minimum standards. Final landscape requirements shall be determined upon review of a site plan, based on CPTED and public safety principles, by the Director of Planning and Community Development in consultation with the Police Chief.

(SF) Adjacent to single-family zones (UL or UM) for buffering purposes. See SMC [15.14.057](#).

(RES) Adjacent to single-family or multi-family zones (UL, UH-900/1800, or MHP) for buffering purposes. See SMC [15.14.057](#).

(Ord. 05-1014 § 4; Ord. 02-1029 § 4; Ord. 00-1037 § 6; Ord. 00-1033 § 9; Ord. 98-1025 § 2; Ord. 95-1016 § 21; Ord. 92-1041 § 1)

Chapter 15.15 Development Standards – Parking and Circulation

(Revised sections include: 15.15.030, 15.15.130)

15.15.030 Parking Space Requirements for Government/Office, Business Uses

USE #	LAND USE	MINIMUM SPACES REQUIRED
GOVERNMENT/OFFICE USES		
071	Social Service Office	1 per 250 sf
072	Public Agency Office	1 per 250 sf
073	Public Agency Yard	1 per 200 sf, plus 1 per 1,000 sf of indoor storage or repair areas
074	Public Archives	1 per employee, plus 1 per 400 sf of waiting/review areas
075	Court	1 per employee, plus 1 per 40 sf of fixed seats or assembly areas
076	Police Facility	1 per employee, plus 1 per 100 sf of public office areas
077	Fire Facility	1 per employee, plus 1 per 100 sf of public office areas
079	Helipad/Airport and Facilities	Helipad: 4 per pad; Airport: 1 per 500 sf of building
080	Utility Use	1 per 250 sf
081	Utility Substation	1 per substation site
082	Financial Institution	1 per 250 sf, plus 5 stacking spaces
083	City Hall	1 space per 250 sf of office area plus 1 per 40 sf of fixed seats or assembly area if a municipal court use is located in City Hall
083.5	Secure Community Transition Facility	1 per employee, plus 0.5 per resident for visitor parking
BUSINESS SERVICES USES		
084	Landscaping	1 per 250 sf of office/storage area
085	Butterfly/Moth Breeding	1 per 250 sf of office/retail area
086	Construction/Trade	1 per 250 sf of office
087	Truck Terminal	1 per 250 sf of office or 1 per employee, whichever is greater
088	Airport Support Facility	1 per 250 sf
089	Warehouse/Storage	1 per 250 sf of office, plus 1 per 3,500 sf of storage areas
090	Professional Office	1 per 300 sf of office building
091	Heavy Equipment Rental	1 per 250 sf of building
092	Misc. Equipment Rental Facility	1 per 250 sf of building

093	Auto Rental/Sales	1 per 300 sf, plus 1 per employee plus a minimum 3,000 sf of display area
094	<u>Commercial (Non-Park-and-Fly) Parking</u> Public/Private Parking	1 per employee (designated)
094.5	<u>Commercial Park-and-Fly Parking</u>	<u>1 per employee (designated)</u>
095	Motor Freight Repair	1 per 300 sf of office, plus 1 per 1,000 sf of indoor repair areas
096	Heavy Equipment Repair	1 per 300 sf of office, plus 1 per 1,000 sf of indoor repair areas
097	R and D/Testing	1 per 300 sf
098	Commercial/Industrial Accessory Uses	1 per 300 sf

(Ord. 02-1029 § 5; Ord. 00-1033 § 12; Ord. 98-1025 § 3; Ord. 95-1016 § 22; Ord. 92-1041 § 1)

15.15.130 Off-Site Parking Location

- A. The City Manager, or designee, may authorize a portion of the required parking for an accessory use (or for up to thirty percent (30%) of the primary use) to be located on a site other than the subject property if:
1. Adequate parking exists for the primary use on the property receiving the additional parking. For the purpose of this section, adequate parking is parking that conforms to current off-street parking requirements for the primary use on the property;
 2. Adequate pedestrian, van or shuttle connection between the sites exists;
 3. The sites are within one (1) mile of each other; and
 4. The site used for off-site parking is zoned to allow commercial (non-park-and-fly) parking or commercial park-and-fly parking ~~public/private parking~~ as a permitted uses.
- B. Nonprofit uses adjacent to each other shall be allowed to share parking, regardless of zoning classification; provided, that:
1. If the shared parking requires an expansion of the parking lot on the property receiving the additional parking, all permit requirements otherwise required for such expansion (such as a conditional use permit and environmental (SEPA) review) must be met.
 2. All requirements of subsections (A)(1) through (3) and (C) of this section are met.

3. Temporary shared parking arrangements between nonprofit uses not exceeding three hundred sixty (360) days shall meet all the requirements of subsections (A)(1) through (3) and subsections (C)(2) through (4) of this section.
- C. Criteria to be used by the City Manager or designee in authorizing off-site parking are:
1. Off-site parking shall be accessed only by employees, not by the general public.
 2. The proposed connections between the sites are safe for pedestrians and vehicles.
 3. The proposed plan is compatible with adjacent uses.
 4. Off-site impacts are negligible or minimized.
 5. A contingency plan is submitted by the applicant and approved by the City that would provide for the parking to be developed on the subject property or established elsewhere if the off-site parking arrangement is no longer available.
- D. Legal documentation is required for the approved, off-site parking location and shall be recorded with the City of SeaTac City Clerk and the Department of Planning and Community Development. Off-site parking may be removed only if alternative parking is provided in conformance with the code and such parking is approved by the City Manager or designee. (Ord. 08-1006 § 1; Ord. 02-1017 § 1; Ord. 95-1012 § 1; Ord. 92-1041 § 1)

Chapter 15.35 Special Standards for the City Center

Sections with proposed amendments include:

Revised Sections

- *15.35.140 Government/Office, Business Uses*
- *15.35.810 Maximum Parking Requirements*
- *15.35.820 Surface Parking*
- *15.35.900 Non-Park-and-Fly Parking Structures*
- *15.35.949 (was 15.35.905 Stand-Alone Parking Structures) Commercial (Non-Park-and-Fly) Parking Structures*
- *15.35.950 Parking Bonus Incentive Program for Commercial (Non-Park-and-Fly) Parking*

New Sections

- *15.35.901 Authority and Application*

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15.35.1000 Commercial Park-and-Fly Development Standards

(see separate document for subsections of this section)

Chapter 15.35 Special Standards for the City Center

15.35.140 Government/Office, Business Uses

ZONES:

P – Park ABC – Aviation Business Center

UM – Urban Medium Density I – Industrial/Manufacturing

UH – Urban High Density O/CM – Office/Commercial Medium

UH-UCR – Urban High-Urban Center Residential O/C/MU – Office/Commercial/Mixed Use

NB – Neighborhood Business T – Townhouse

CB-C – Urban Center

P – Permitted Use; C – Conditional Use Permit

USE #	LAND USE	ZONES										
		P	UM	UH	UH-UCR	NB	CB-C	ABC	I	O/CM	O/C/MU	T
GOVERNMENT/OFFICE USES												
071	Social Service Office			C	P	P	P	P	P	P	P	
072	Public Agency Office	P		P	P	P	P	P	P	P	P	
073	Public Agency Yard	C(2)					C	C	P	C	C	
074	Public Agency Archives	C(3)				C	P	P	P	P	P	
075	Court						P	P	P	P	P	
076	Police Facility	P	P	P	P	P	P	P	P	P	P	
077	Fire Facility	P	P	P	P	P	P	P	P	P	P	
079	Helipad/Airport and Facilities								P			
080	Utility Use		C	C	C	C	C	C	P	C	C	
081	Utility Substation			C	C	C	C	C	P	C	C	
082	Financial Institution				P(4)	P	P	P	P	P	P	
083	City Hall				P(4)	P	P	P	P			
083.5	Secure Community Transition Facility						C(7)	C(7)	C(7)	C(7)		
BUSINESS SERVICES USES												
086	Construction/Trade						C	P(1)	P	C		
087	Truck Terminal							P(1)	P			

088	Airport Support Facility							P				
089	Warehouse/Storage				C			P	P	C(1)		
090	Professional Office		P(4)	P(4)	P	P	P	P	P	P	P	
091	Heavy Equipment Rental							C	P			
092	Misc. Equipment Rental Facility				C	C			P	P(1)		
093	Auto Rental/Sales						P(1)	P(1)	P	C(1)		
094	<u>Commercial (Non-Park-and-Fly) /Public/Private Parking Lot</u>				C(5,6,8)	P(5,6,8)	P(5,6,8)	P(5,6,8)	P(5,6,8)	C(5,6,8)		
094.5	<u>Commercial Park-and-Fly Parking</u>						P (5,6)			C (5,6)		
095	Motor Freight Repair								P			
096	Heavy Equipment Repair								P			
097	R and D/Testing				C	C	P	P	P	C		
098	Commercial/Industrial Accessory Uses				P	C	P	P	P	C		

(1) Accessory to primary use not to exceed twenty percent (20%) of primary use.

(2) A public agency yard located on property within the park zone may be used as a combined maintenance facility for park and nonpark purposes; provided, that the facility shall be no more expansive than that which is reasonably expected to be needed for park maintenance when park facilities are fully developed.

(3) A public archives facility located on property within the park zone is limited to existing structures.

(4) Permitted as part of a mixed use development, as described in SMC [15.35.610](#).

(5) Commercial (Non-Park-and-Fly) Parking ~~/Public/private parking lots (park n fly) and Commercial Park-and-Fly Parking~~ are only permitted within a structure. Please see SMC [15.35.820](#) for provisions regarding ~~public/private~~ surface parking for Commercial Park-and-Fly Parking lot as an interim use. ~~Please see SMC 15.35.900 for parking structure design and development standards.~~

(6) See SMC 15.35.1000 and its subsections for standards for Commercial Park-and-Fly Parking developments. ~~Public/private parking lot (park n fly) structures are permitted up to three hundred (300)~~

~~spaces as a stand-alone structure. (See SMC 15.35.905 for requirements regarding stand-alone structures.) Additional spaces may be added only via the incentive method defined in SMC 15.35.950.~~

(7) Secure community transition facilities are subject to the CUP-EPF siting process (SMC 15.22.035).

~~(8) Public/private parking lots shall only be allowed in one (1) parking structure per development site.~~

(Ord. 10-1026 § 1 (Exh. A); Ord. 08-1023 § 2; Ord. 05-1002 § 1; Ord. 02-1029 § 10; Ord. 99-1050 § 8)

15.35.810 Maximum Parking Requirements

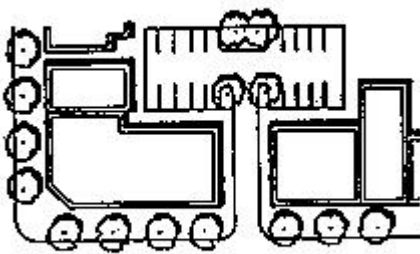
LAND USE	MAXIMUM SPACES ALLOWED*
Residential Uses	
Single Attached Dwelling/Duplex/Townhouse	2 per dwelling unit
Multi-Family Housing	1 per bedroom, up to 2 per dwelling unit maximum***
Senior Housing	1 per unit***
Boarding House/Bed and Breakfast	1 per bedroom, plus 2 for residents
Transitional Housing	1 per 2 bedrooms***
Convalescent/Community Residential Facility	1 per bed***
Rest/Convalescent Center/Nursing Home (24 hr. care)	1 per 3 beds***
College Dormitory	1 per bedroom unit
Residential Congregate Care	0.35 per bedroom
Recreational/Cultural Uses	
Conference/Convention Center	5 per 1,000 SF of building area
Library/Museum/Cultural Facility	4 per 1,000 SF of building area
Community Center/Recreation Center	4 per 1,000 SF of building area
Sports/Fitness/Health Club	4 per 1,000 SF of building area
Theater	0.75 per fixed seat, plus 1 per employee
General, Educational and Health Services Uses	
General Service Uses	4 per 1,000 SF of building area
Educational Uses	1 per employee, plus 1 per 2 students
Health Services Uses	4 per 1,000 SF of building area
Government/Office, Business Uses	
Business Service/Office Uses	5** per 1,000 SF of building area
Retail/Commercial Uses	
Hotel/Motel and Associated Uses	1 per bedroom, plus the following for associated uses:
• with restaurant/lounge/bar	• 4 per 1,000 SF of building area
• with banquet/meeting room	• 5 per 1,000 SF of gross building area
Retail Uses	4 per 1,000 SF of leasable space
Manufacturing Uses	1 per employee, plus 2 per 1,000 SF of building area
<u>Commercial (Non-Park-and-Fly) Parking</u>	See SMC 15.35.820 Surface Parking and 15.35.949 Special Standards for Commercial (Non-Park-and-Fly) Parking
<u>Public/Private Parking Lot</u>	15.35.140, footnote (5), (6) and (8) for information regarding public/private parking
<u>Commercial Park-and-Fly Parking</u>	See SMC 15.35.1013, Maximum Number of Parking Stalls

15.35.820 Surface Parking –~~Interim Use~~

BA. Location of Surface Parking ~~Lots.~~

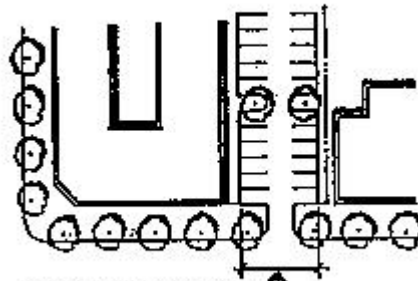
1. No parking shall be located between the building and the front property line, other than a driveway for passenger loading and off-loading only in conformance with SMC 15.35.340 and approved by the Director of Planning and Community Development. Surface parking shall be located behind a building or to the side of a building.
2. Parking located next to a building and within forty (40) feet of the front property line shall not occupy more than the width of two (2) lengthwise parking stalls and one (1) travel lane, or sixty-two (62) feet, whichever is less.
3. On corner lots, no parking shall be located between the building and either of the two (2) front property lines. If a parcel abuts more than two (2) public or private streets, no parking shall be located between the building and the front property line abutting the two (2) public and/or private streets with the highest classification.

PARKING TO REAR OF BLDG.



NO PARKING ON CORNERS

PARKING TO SIDE OF BLDG.



MAXIMUM WIDTH ↕

AB. ~~Commercial/Public/Private~~ Surface Parking for Commercial (Non-Park-and-Fly) Parking as an Interim Use. ~~Commercial (Non-Park-and-Fly) /Public/private~~ surface parking, as the main activity on a site, may only be allowed as an interim use subject to the following conditions:

1. A development agreement specifying additional conditions as needed.

15.35.900 Non-Park-and-Fly Parking Structures

Purpose: Design parking structures to blend in with adjacent development. Emphasize design features that minimize the obtrusiveness of the parking use and encourage architectural compatibility with adjacent development. (Ord. 99-1050 § 8)

15.35.901 Authority and Application

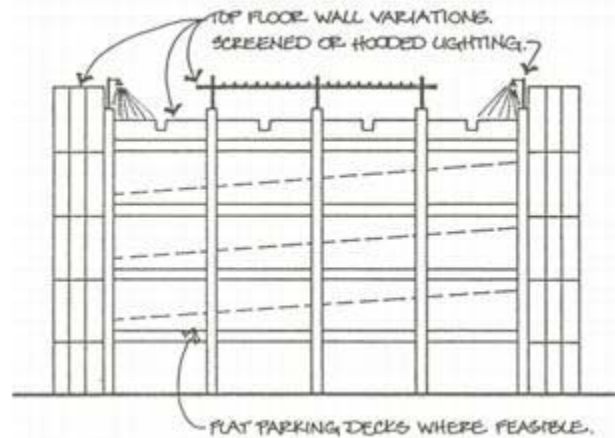
The provisions of sections 15.35.900 through 15.35.947 shall apply to all parking structures located within the City Center, except for those which provide spaces for Commercial Park-and-Fly Parking.

15.35.910 Parking Structure Design

The following parking structure design standards shall be in addition to or, in some cases, supersede applicable design standards in other sections in this chapter.

- A. Parking decks should be flat where feasible. At a minimum, a majority of both the ground floor and top parking decks shall be required to be flat, as opposed to continuously ramping.
- B. External elevator towers and stair wells shall be open to public view, or enclosed with transparent glazing.
- C. Lighting on and/or within multi-level parking structures shall be screened, hooded or otherwise limited in illumination area so as to minimize excessive “light throw” to off-site areas.
- D. Parking structure top floor wall designs must conform to one (1) or more of the following options:
 1. Top Floor Wall with Architectural Focal Point. A top floor wall focal point refers to a prominent wall edge feature such as a glazed elevator and/or stair tower, or top floor line trellis structure.
 2. Top Floor Wall Line Variation.

- a. Projecting Cornice: Top floor wall line articulated through a variation or step in cornice height or detail. Cornices must be located at or near the top of the wall or parapet.
- b. Articulated Parapet: Top floor wall line parapets shall incorporate angled, curved or stepped detail elements.



(Ord. 99-1050 § 8)

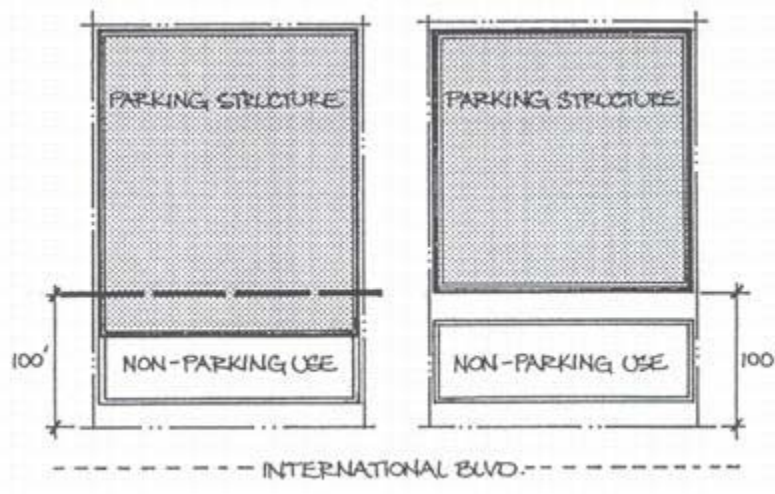
15.35.915 Parking Structure Placement and/or Setbacks

- A. Parking Structures on Properties Adjacent to International Boulevard. Except as otherwise specified below, the provisions of this subsection shall supersede the setback standards specified in SMC 15.35.310. No parking structures shall be located within one hundred (100) feet of the International Boulevard ROW, except as specified below:
 1. Parking structures located behind or adjacent to additional nonparking buildings facing International Boulevard may be located in a manner that meets developer needs, within the setback requirements (SMC 15.35.310) and other applicable building codes, except that portions of parking garages exceeding the height of fronting buildings shall meet the one hundred (100) foot requirement specified above.
 2. Parking structures may be integrated into buildings built within the maximum setback (SMC 15.35.310); provided, that a retail/commercial, service, office, or residential use, or a combination of these uses, comprises the building's face for its full height on International Boulevard.
 3. The entire space within the one hundred (100) foot setback area may be developed as a public plaza to a level of design accepted by the Director of Planning and Community Development.

—For corner lots on International Boulevard, the parking structure must be faced with other uses as specified in subsection (A)(2) of this section on all sides adjacent to public

and/or private streets for a distance of one hundred (100) feet from International Boulevard.

- B. Parking Structures on Properties Adjacent to All Other City Center Public and/or Private Streets. Parking structures shall be located within the maximum front yard setback, as specified in SMC 15.35.310, or built to the side or rear of the subject property when located behind or to the side of additional buildings on-site.



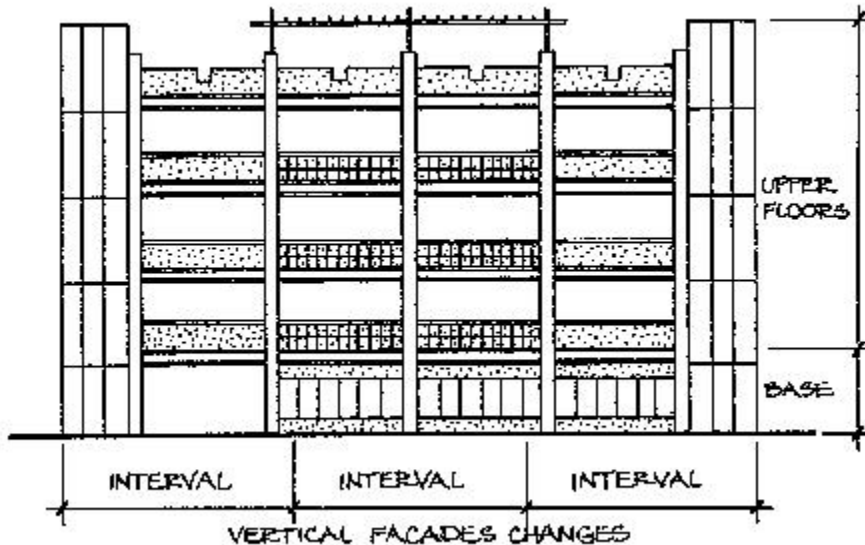
(Ord. 99-1050 § 8)

15.35.920 Parking Structure Character and Massing

Parking structure elevations over one hundred fifty (150) feet in length shall incorporate vertical and/or horizontal variation in setback, material or fenestration design along the length of the applicable facade, in at least one (1) or more of the following ways:

- A. Vertical Facade Changes. Incorporate intervals of architectural variation at least every eighty (80) feet over the length of the applicable facade, such as:
1. Varying the arrangement, proportioning and/or design of garage floor openings;
 2. Incorporating changes in architectural materials;
 3. Projecting forward or recessing back portions or elements of the parking structure facade;
- B. Horizontal Facade Changes. Designed to differentiate the ground floor from upper floors, such as:
1. Stepping back the upper floors from the ground floor parking structure facade;
 2. Changing materials between the parking structure base and upper floors; and/or

3. Including a continuous cornice line or pedestrian weather protection element between the ground floor and upper floors.



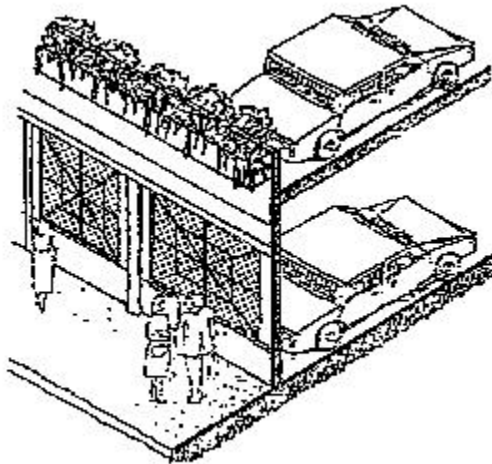
(Ord. 99-1050 § 8)

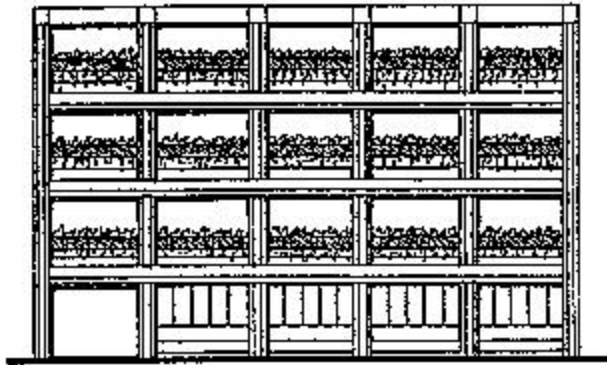
15.35.930 Minimizing Views Into the Parking Structure Interior

Facades of parking structures facing a public street or pedestrian way as defined by SMC 15.35.220 shall be designed without continuous horizontal parking floor openings.

- A. For portions of parking structures without a ground floor retail/commercial use, the following building facade landscaping is required:
1. Five (5) foot wide facade landscape strip consisting of:
 - a. A mix of evergreen shrub groupings spaced no more than four (4) feet apart that do not exceed a height of six (6) feet at maturity;
 - b. Ground cover; and
 - c. Seasonal displays of flowering annual bedding plants.
- B. Any portion of a parking structure ground floor with exposed parking areas adjacent to a public or private street shall minimize views into the parking structure interior through one (1) or more of the following methods which are in addition to the above facade landscaping strip:

1. Decorative trellis work and/or screening as architectural elements on the parking structure facade, without compromising the open parking structure requirements of the Building Code; and/or
 2. Glass window display cases incorporated into ground floor walls built between two (2) structural pillars. Glass window display cases shall be at least two (2) feet deep, begin twelve (12) to thirty (30) inches above the finished grade of the sidewalk, and cover at least sixty percent (60%) of the area between two (2) pillars.
- C. Upon conversion of portions of a parking structure ground floor to a retail/commercial use, the Director of Planning and Community Development may approve the removal of initially installed ground floor screening material in order to allow maximum visibility and access to the converted portions of the parking structure ground floor.
- D. In addition to the above, minimize views into the upper floors of parking structures through one (1) or more of the following methods:
1. The use of planters integrated into the upper floors of parking structure facade design;
 2. Decorative trellis work and/or screening as architectural elements on the parking structure upper floor facades; and/or
 3. Upper parking floors designed as a pattern of window-like openings on the parking structure facade.

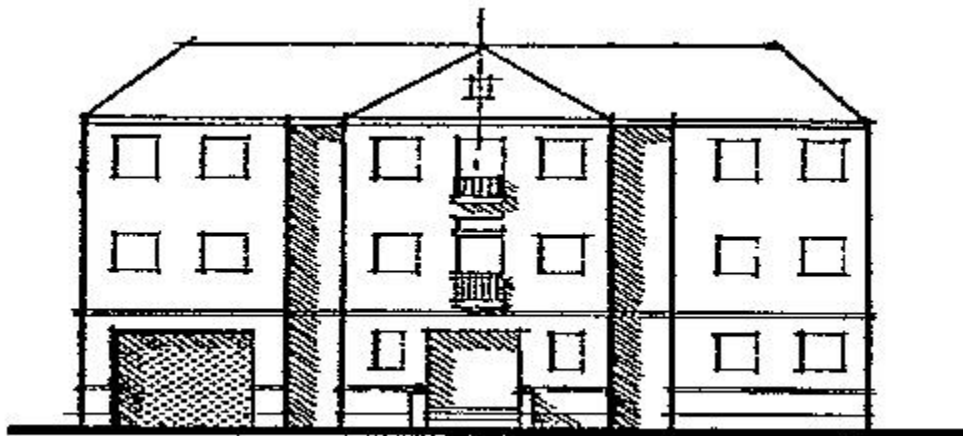


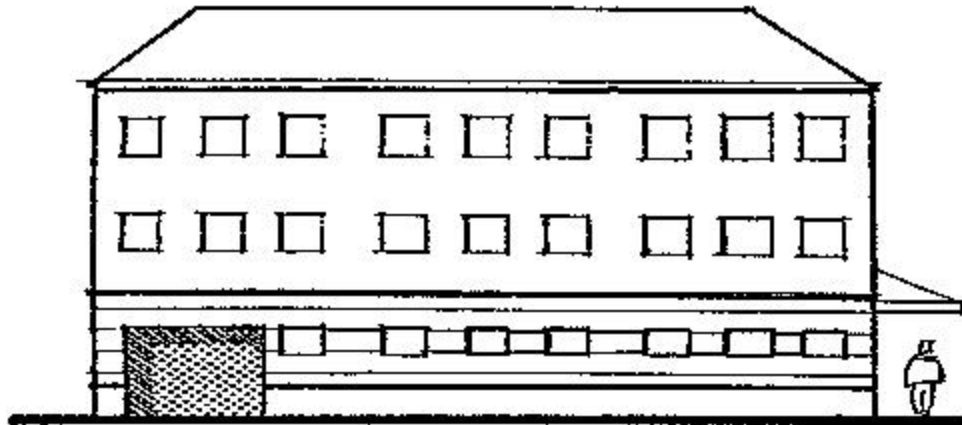


(Ord. 04-1008 § 4; Ord. 99-1050 § 8)

15.35.940 Parking Floors Located Under or Within Buildings

- A. Parking located under or within buildings shall subordinate the garage entrance to the pedestrian entrance in terms of prominence on the street, location and design emphasis.
- B. Parking at grade under a building shall be completely enclosed within the building or wholly screened through any combination of walls, decorative grilles, or trellis work with landscaping.



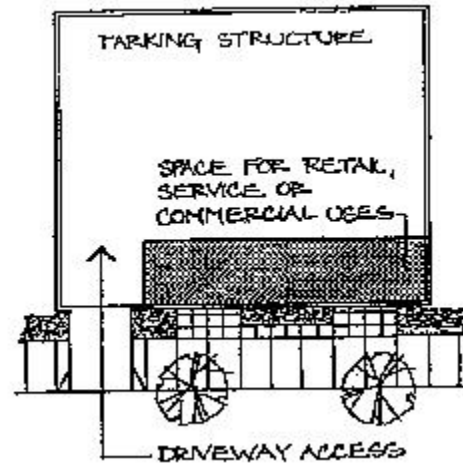
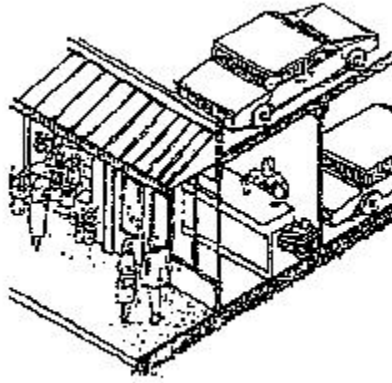


(Ord. 99-1050 § 8)

15.35.945 Ground Floor Uses in Parking Structures

- A. Parking structures shall be designed so that a minimum of fifty percent (50%) of the length of the exterior ground floor facade(s) with existing or projected adjacent foot traffic, excluding vehicle entrances and exits, provides ground floor area either built out as, or convertible to, retail/commercial or service uses. The applicable ground floor area shall extend in depth a minimum of twenty (20) feet from the exterior parking structure facade; provided, that the minimum required may be averaged, with no depth less than fifteen (15) feet.
1. The minimum clear interior ceiling height standard for the retail/commercial or service use portion of parking structures shall be ten (10) feet for all street level building space.
 2. Parking structure ground floors shall include fire suppressing sprinkler systems at the time of construction.
- B. At the time of construction, a minimum of four hundred (400) square feet of leasable retail/commercial or service space shall be constructed and made available for occupancy. The remainder of the area necessary to fulfill the minimum retail/commercial or service use requirement specified in subsection (A) of this section, but not included at the time of construction, shall employ window display cases, as specified in SMC 15.35.930(B)(2) to meet the transparency requirements of SMC 15.35.510.
- C. Parking structures with ground floor retail/commercial or service uses will be granted an additional parking allowance as follows:

1. The number of parking spaces displaced by the portion of the parking structure ground floor designed for retail/commercial or service uses may be added to the maximum number of allowed parking spaces established for on-site land uses.



(Ord. 99-1050 § 8)

15.35.947 Parking Structure Lighting

Lighting of parking structures shall be provided pursuant to Chapter 17.28 SMC, Parking Structures. (Ord. 03-1033 § 10)

15.35.94905 ~~Stand-Alone~~ Commercial (Non-Park-and-Fly) Parking Structures

~~Stand-alone~~ Commercial (non-park-and-fly) parking structures ~~allowed under SMC 15.35.140, Use No. 094,~~ shall comply with the following minimum requirements:

- A. Only one (1) ~~stand-alone~~ parking structure for commercial (non-park-and-fly) uses shall be allowed per development site. (~~Also See~~ SMC 15.10.175.03 for the definition of “development site”.)
- B. A ~~stand-alone commercial (non-park-and-fly)~~ parking structure is limited to not more than three hundred (300) parking stalls for commercial (non-park-and-fly) use unless additional spaces are allowed under SMC 15.35.950.
- C. ~~Stand-alone parking~~ Commercial (non-park-and-fly) parking structures implementing the parking incentive of SMC 15.35.950(B)(3) shall locate all required off-street parking spaces for the retail/commercial, service, or residential use(s) adjacent to such uses. The spaces shall be reserved and clearly designated for the customers of those uses.
- D. No ~~stand-alone commercial (non-park-and-fly)~~ parking structure shall be allowed on a development site specifically created through a commercial/industrial subdivision.

- E. Design features for ~~stand-alone commercial (non-park-and-fly)~~ parking structures shall comply with the requirements of SMC 15.35.100 and 15.35.900 through ~~15.35.945~~ 15.35.950. (~~Ord. 05-1002 § 5~~)

15.35.950 Parking Bonus Incentive Program for Commercial (Non-Park-and-Fly) Parking

- A. A parking allowance bonus, beyond the maximum parking specified in SMC 15.35.949(B) ~~15.35.810~~, will be granted to those developments which provide retail/commercial or service space beyond the requirements of SMC 15.35.620, or a public benefit in the form of:
1. Dedicated public right-of-way, in an arrangement and amount per parcel that conforms to the City Center vehicular and pedestrian access plan; and/or
 2. Publicly accessible on-site open space greater than the minimum ten percent (10%) of net site area required, or an equivalent monetary contribution to the City Center open space fund; and/or
 3. A water feature or public art display incorporated into publicly accessible on-site open space, as approved by the Director of Planning and Community Development.
- B. The formula for calculating parking bonuses above maximum allowed for on-site land uses shall be as follows:
1. One (1) additional parking stall will be awarded for each one hundred fifty (150) square feet of interconnected public right-of-way dedicated according to the City Center vehicular and pedestrian access plan;
 2. One (1) additional parking stall will be awarded for each one hundred fifty (150) square feet of publicly accessible on-site open space greater than the minimum ten percent (10%) of net site area required. To receive parking bonus in lieu of additional on-site open space, developments may contribute to the City Center open space fund in increments of equivalent monetary value;
 3. Except for hotel/motel uses, one (1) additional parking stall will be awarded for each two hundred fifty (250) square feet of retail/commercial, service, or residential space, in addition to the minimum ground floor retail/commercial or service space required under SMC 15.35.620, included on the same site as part of a mixed use development at the time of construction;
 4. Hotel/motel uses shall be awarded one-half (0.5) parking space per hotel/motel unit, in addition to the minimum ground floor retail/commercial or service space required under SMC 15.35.620, included on the same site as part of a mixed use development at the time of construction; and/or
 5. Up to sixty (60) additional parking stalls may be awarded for a water feature or public art display of equivalent value incorporated into publicly accessible on-site open space, as approved by the Director of Planning and Community Development.

Value shall be determined by the per-square-foot market value of the underlying land multiplied by the square footage of the additional parking stalls. (Ord. 09-1007 § 1; Ord. 08-1027 § 1; Ord. 08-1014 § 1; Ord. 99-1050 § 8)

Model 1: Sections 15.35.1000-15.35.1053*Amended sections include: 15.35.1000-15.35.1053)***15.35.1000 Commercial Park-and-Fly Parking Developments****15.35.1010 905—Parking Standards~~Stand-Alone Parking Structures~~**15.35.1011 Parking Structures Per Development Site15.35.1012 Maximum Number of Parking Stalls15.35.1013 Off-Street Parking for Uses Other than Commercial Park-and-Fly Parking**15.35.1020 950 Parking Bonus Incentive Program for Commercial Park-and-Fly Parking Developments****15.35.1030 Retail/Commercial/Service Space Requirements and Dimensional Standards**15.35.1031 Charts User Guide15.35.1032 Retail/Commercial/Service Space Requirements and Front Yard Setbacks Standards Chart15.35.1033 Dimensional Standards Chart~~15.35.350~~ Projects on or Near the Edge of a UL or UM Residential Zone~~15.35.310~~ Building Placement/Setbacks~~15.35.915~~ Parking Structure Placement and/or Setbacks~~15.35.945~~ Ground Floor Uses in Parking Structures**15.35.1040 ~~15.35.300~~ Site Design~~Planning and Building Orientation~~**15.35.1041 Building Orientation~~15.35.1042~~ 210 Vehicular Circulation Requirements~~15.35.1043~~ 220 Pedestrian Circulation Requirements~~15.35.1044~~ 400 Open Space and Amenities15.35.1045 Landscaping~~15.35.1046~~ 335 The Layout and Width of Streetfront Pedestrian Zone~~15.35.1047~~ 340 Driveway Entrances~~15.35.320~~ Development Abutting Two (2) or More Street Frontages~~15.35.345~~ Exterior Lighting~~15.35.410~~ Minimum Open Space Area Required~~15.35.420~~ Front Yard Open Space~~15.35.430~~ Incorporating Bow Lake as a Focal Point~~15.35.330~~ Relation to Adjacent Development

15.35.1050~~15.35.500~~ ___ Building Design

~~15.35.910_ Parking Structure Design~~

15.35.1051 All Structures

15.35.1052 Non-Parking Structures

15.35.1053 Parking Structures

15.35.1000s Park-and-Fly Developments Standards (Model 1)

15.35.1000 Commercial Park-and-Fly Parking Developments

A. Authority and Applications

The provisions of sections 15.35.1000 through 15.35.1053 shall apply to all commercial park-and-fly parking developments, including any project incorporating a commercial park-and-fly parking use, located within the City Center . These regulations shall supersede existing regulations in SMC 15.35 and elsewhere in SMC Title 15 when in conflict with these sections.

B. Purpose

The following standards are intended to allow for the development of park-and-fly parking developments within the City Center/central business district by promoting integrated development and minimizing the visual impact of parking as a dominant land use.

15.35.1010 ~~15.35.905~~ Parking Standards Stand-Alone Parking Structures

~~Stand-alone parking structures allowed under SMC 15.35.140, Use No. 094, shall comply with the following minimum requirements:~~ Commercial park-and-fly parking developments shall comply with the minimum requirements established in sections 15.35.1011 through 15.35.1013.

15.35.1011A Parking Structures Per Development Site

Only one (1) ~~stand-alone~~ commercial park-and-fly -parking structure shall be allowed per development site. ~~(Also see SMC 15.10.175.03.)~~

~~D.— No stand-alone parking structure shall be allowed on a development site specifically created through a commercial/industrial subdivision.~~

15.35.1012 Maximum Number of Parking Stalls

A ~~stand-alone~~ parking structure is limited to not more than one thousand (1,000) three hundred (300) parking stalls for commercial park-and-fly parking uses unless additional spaces are allowed under SMC 15.35.1020~~950~~, Parking Bonus Incentive Program for Commercial Park-and-Fly Parking.

~~E.— Design features for stand-alone parking structures shall comply with the requirements of SMC 15.35.100 and 15.35.900 through 15.35.945. (Ord. 05-1002 § 5)~~

15.35.1013 Off-Street Parking for Uses Other than Commercial Park-and-Fly Parking

~~C.— Stand alone parking structures implementing the parking incentive of SMC 15.35.950(B)(3) shall locate all required providing required off-street parking spaces for the uses other than commercial park-and-fly parking, including retail, commercial, office, service, public, or residential use(s) adjacent to such uses. The spaces, shall clearly be reserved and clearly designated all required spaces for the customers of those said use(s).~~

15.35.1020950 -Parking Bonus Incentive Program for Commercial Park-and-Fly Parking Developments

~~A.— Additional parking spaces for park-and-fly commercial parking A parking allowance bonus, beyond the maximum allowance parking specified in SMC 15.35.1012, Maximum Number of Parking Stalls, can be earned by providing certain benefits, as set forth in the following chart. 810, will be granted to those developments which provide retail/commercial or service space beyond the requirements of SMC 15.35.620, or a public benefit in the form of:~~

ADDITIONAL PARKING SPACES	BENEFIT
1	Public Right-of-Way: 1.— One (1) additional parking stall will be awarded for each o One hundred fifty (100+50) square feet of interconnected public right-of-way dedicated according to the City Center vehicular and pedestrian access plan.;
1	Open Space: 2.— One (1) additional parking stall will be awarded for each o One hundred fifty (100+50) square feet of publicly accessible on-site open space greater than the minimum fiveten percent (5+0 %) of net site area required. To receive parking bonus in lieu of additional on-site open space, developments may contribute to the City Center open space fund in increments of equivalent monetary value.;
1	Retail/Commercial/Service/Residential Space: Twenty-five two hundred fifty (25250) square feet of retail/commercial, service, or residential space, in addition to the minimum ground floor retail/commercial or service space required under SMC 15.35.10322620:—, included on the same site

	<p>as part of a mixed-use development at the time of constructionoccupancyoccupancy;</p> <p>(This bonus does not apply to hotel/motel uses</p> <p>Except for hotel/motel uses, one (1) additional parking stall will be awarded for each.)</p>
1	<p>Hotel/Motel Units: 4.—Hotel/motel uses shall be awarded one half (0.5) parking space perA hotel/motel unit, in addition to the minimum ground floor retail/commercial or service space required under SMC 15.35.10322620, included on the same site as part of a mixed-use development at the time of constructionoccupancy; and/or</p>
60	<p>Water Feature/Public Art: 5.—Up to sixty (60) additional parking stalls may be awarded forA water feature or public art display of equivalent value incorporated into publicly accessible on-site open space, as approved by the Director of Planning and Community and Economic Development. Value shall be determined by the per-square-foot market value of the underlying land multiplied by the square footage of the additional parking stalls.</p>

15.35.1030 Retail/Commercial/Service Space Requirements and Dimensional Standards

Commercial park-and-fly parking developments shall comply with the retail/commercial/service space and dimensional requirements as set forth in sections 15.35.1031 through 15.35.1033 below.

15.35.945 Ground Floor Uses in Parking Structures

~~A.—Parking structures shall be designed so that a minimum of fifty percent (50%) of the length of the exterior ground floor facade(s) with existing or projected adjacent foot traffic, excluding vehicle entrances and exits, provides ground floor area either built out as, or convertible to, retail/commercial or service uses. 1.—The minimum clear interior ceiling height standard for the retail/commercial or service use portion of parking structures shall be ten (10) feet for all street level building space.~~

~~2.—Parking structure ground floors shall include fire suppressing sprinkler systems at the time of construction.~~

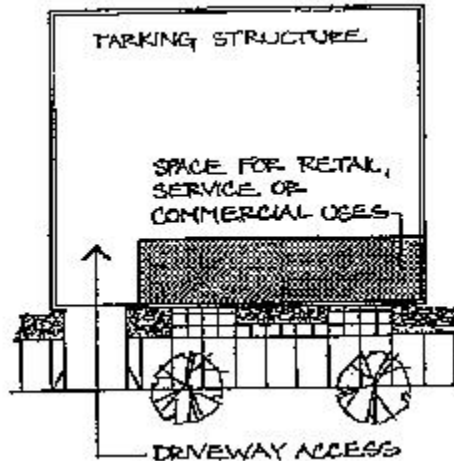
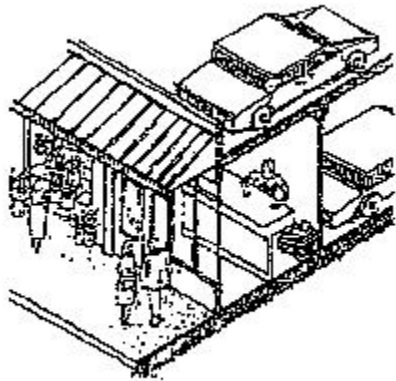
~~B.—At the time of construction, a minimum of four hundred (400) square feet of leasable retail/commercial or service space shall be constructed and made available for occupancy. The remainder of the area necessary to fulfill the minimum retail/commercial or service use requirement specified in subsection (A) of this section,~~

but not included at the time of construction, shall employ window display cases, as specified in SMC 15.35.930(B)(2) to meet the transparency requirements of SMC 15.35.510.

The applicable ground floor area shall extend in depth a minimum of twenty (20) feet from the exterior parking structure facade; provided, that the minimum required may be averaged, with no depth less than fifteen (15) feet.

C. Parking structures with ground floor retail/commercial or service uses will be granted an additional parking allowance as follows:

1. The number of parking spaces displaced by the portion of the parking structure ground floor designed for retail/commercial or service uses may be added to the maximum number of allowed parking spaces established for on-site land uses.



15.35.1031 Charts User Guide

A. About the Standards Charts

The charts in this section list general standards for commercial park-and-fly parking developments. The first chart lists the retail/commercial/service space requirements and the front yard setbacks for the five (5) types of commercial park-and-fly buildings. The second chart lists dimensional standards for the zones where commercial park-and-fly parking development is permitted: Community Business-Urban Center and Office/Commercial Medium.

B. How to use the Standards Charts

The dimensional standards are listed vertically along the left hand side and the zones are listed horizontally across the top. In addition to the numerical value for the standard, the following symbols may appear in a cell:

1. n: User should refer to the regulations listed in the far right column for that development standard.

2. --: A dimensional standard does not exist for that zone.

C. Additional regulations according to the standard

The column on the far right lists sections of the code which need to be referenced for additional regulations which apply to that particular standard. If the regulation is not preceded by an “n”, the regulation applies to all zones.

D. Zone Abbreviations

1. CB-C: Community Business-Urban Center

2. O/CM: Office/Commercial Medium

15.35.1032 Retail/Commercial/Service Space Requirements and Front Yard Setbacks Standards Chart

Park-and-fly parking developments shall choose one of the five building types listed in this chart. Setbacks are determined by whether or not the property fronts on International Boulevard and by the amount of ground floor retail/commercial/service space that is provided. See the figures following the chart for assistance.

SYMBOLS

- n: User should refer to the regulations listed in the far right column for that specific standard.
- : A dimensional standards does not exist for that zone.

DEVELOPMENT STANDARDS		BUILDING TYPE					ADDITIONAL REGULATIONS
		1	2	3	4	5	
PROPERTY FRONTING ON INTERNATIONAL BOULEVARD		<u>Yes</u>	<u>Yes</u>	<u>Yes</u>	<u>No</u>	<u>Yes-n</u>	n: Corner lot with International Boulevard. Regulations for Type 5 apply to building façade fronting on non-International Boulevard street. The façade fronting on International Boulevard shall conform to the regulations of Type 1, 2, or 3.
MINIMUM GROUND FLOOR RETAIL/COMMERCIAL/SERVICE SPACE FRONTING ON A STREET		<u>400 sf-n</u>	<u>Full building-n1</u>	<u>75%-n2, n3</u>	<u>15%-n, n2, n3</u>	<u>20%-n, n2, n3</u>	n: Shall be built as part of the parking structure n1: A retail/commercial/service building at minimum the same height and width of the parking structure shall be built in between International Boulevard and the parking structure. n2: Percentage of parking structure's ground floor facade length fronting on a street, excluding vehicle entrances and exits n3: Shall not be less than 400 square feet
MINIMUM RETAIL/COMMERCIAL/SERVICE SPACE DEPTH		<u>30'</u>	<u>30'</u>	<u>30'</u>	<u>30'</u>	<u>30'</u>	
FRONT YARD SETBACK, COMMERCIAL/RETAIL/SERVICE	MINIMUM	<u>0'</u>	<u>0'</u>	<u>0'</u>	<u>10'</u>	<u>10'</u>	If the landscape buffer exceeds the minimum setback, the buffer becomes the minimum setback.
	MAXIMUM	<u>20'-n, n1</u>	<u>20'-n1</u>	<u>20'-n1</u>	<u>10'</u>	<u>10'</u>	n: Does not apply to initial retail/commercial/service space built as part of the parking structure n1: At least 50% of the building's front facade shall be located within the maximum front yard setback
FRONT YARD SETBACK, PARKING STRUCTURE	MINIMUM	<u>100'</u>	<u>n</u>	<u>75'</u>	<u>10'</u>	<u>10'</u>	If the landscape buffer exceeds the minimum setback, the buffer becomes the minimum setback. n: Must be built behind, and fully screened by, a retail/commercial/service building
	MAXIMUM	<u>--</u>	<u>n</u>	<u>--</u>	<u>10'</u>	<u>10'</u>	n: Must be built behind, and fully screened by, a retail/commercial/service building

15.35.1033 Dimensional Standards Chart

SYMBOLS

n: User should refer to the regulations listed in the far right column for that specific standard.

--: A dimensional standards does not exist for that zone.

DEVELOPMENT STANDARDS		ZONE		ADDITIONAL REGULATIONS
		CB-C	O/CM	
FRONT YARD SETBACK		See Retail/Commercial/Service Space Requirements and Front Yard Setbacks		
SIDE YARD SETBACK, MINIMUM	NOT ADJACENT TO UL OR UM ZONE	0'	5'	If the landscape buffer exceeds the minimum setback, the buffer becomes the minimum setback.
	ADJACENT TO UL OR UM ZONE	20'	20'	
REAR YARD SETBACK, MINIMUM	NOT ADJACENT TO UL OR UM ZONE	0'	5'	If the landscape buffer exceeds the minimum setback, the buffer becomes the minimum setback.
	ADJACENT TO UL OR UM ZONE	20'	20'	
LOT COVERAGE, MAXIMUM		75%	75%	
STRUCTURE HEIGHT	MINIMUM	<u>18'</u>	<u>18'</u>	<u>One (1) story structures shall be a minimum of eighteen (18) feet and have the appearance of two (2) story structures.</u>
	MAXIMUM	FAA/Fire Standards-n	45'	Properties abutting a UL zone shall incorporate a maximum building height of 35' within 60' of the abutting property line, relative to the base elevation of the adjacent UL parcel(s) where that base elevation is higher than the base elevation of the proposed project; provided, that the overall height of any structure shall not exceed the maximum structure height specified above n: Limited by FAA and Fire Department regulations. Requires written certification from the FAA.

~~2.— A minimum side and/or rear yard building setback of twenty (20) feet shall apply if the side or rear property boundaries are adjacent to a UL zone. Side/rear yard landscaping shall occupy all or part of the required building setback, as specified by land use in SMC 15.14.060.~~

~~B.— Properties abutting a UM zone shall maintain a minimum side and/or rear yard building setback of twenty (20) feet, if the side or rear yard property boundaries are adjacent to a UM zone. Side/rear yard landscaping shall occupy all or part of the required building setback, as specified by land use in SMC 15.14.060.~~

15.35.310 Building Placement/Setbacks

~~A.— For City Center properties zoned UH-UCR, CB-C, and O/CM, the maximum front yard setbacks shall be as follows, except as provided under SMC 15.35.915, Parking Structure Placement and/or Setbacks:~~

~~1.— Twenty (20) feet adjacent to International Boulevard, for at least fifty percent (50%) of the building's front facade;~~

~~2.— Ten (10) feet adjacent to all other City Center public and/or private streets, as specified in the City Center Plan (see City Center Plan, Figures 5.1—5.1b), for at least fifty percent (50%) of the building's front facade.~~

~~B.— The front facade of the primary building(s) on-site shall be oriented toward the front property line, with the main pedestrian entrance(s) located on this front facade. Additional building entrances may be oriented toward the rear or side of the building for access to and from parking lots.~~

~~C.— At least fifty percent (50%) of the building's front facade shall be located within the maximum front yard setback, as specified in subsections (A)(1) and (2) of this section. The remaining portions of the front facade may be stepped back a maximum of twenty (20) feet more than the established maximum setback, as approved by the Director of Planning and Community Development, for the purpose of accommodating public open space, porte cocheres, or recessed building entries.~~

~~D.— Building placement and setback shall be arranged to accommodate the front yard open space requirement as specified in SMC 15.35.420.~~

~~E.— For projects in the CB-C zone, where the side and rear setbacks in SMC 15.13.010 conflict with the required landscaping in SMC 15.14.060 and 15.14.120, the side and rear yard setbacks in SMC 15.13.010 shall supersede. This shall not apply where side and rear property lines abut a residential zone. (Ord. 08-1026 § 1; Ord. 99-1050 § 8)~~

~~15.35.915 Parking Structure Placement and/or Setbacks~~

~~A. Parking Structures on Properties Adjacent to International Boulevard. Except as otherwise specified below, the provisions of this subsection shall supersede the setback standards specified in SMC 15.35.310. No parking structures shall be located within one hundred (100) feet of the International Boulevard ROW, except as specified below:~~

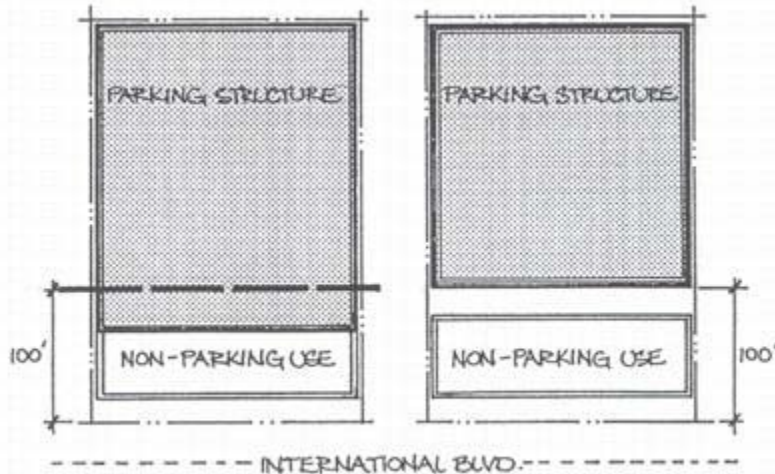
~~1. Parking structures located behind or adjacent to additional nonparking buildings facing International Boulevard may be located in a manner that meets developer needs, within the setback requirements (SMC 15.35.310) and other applicable building codes, except that portions of parking garages exceeding the height of fronting buildings shall meet the one hundred (100) foot requirement specified above.~~

~~2. Parking structures may be integrated into buildings built within the maximum setback (SMC 15.35.310); provided, that a retail/commercial, service, office, or residential use, or a combination of these uses, comprises the building's face for its full height on International Boulevard.~~

~~3. The entire space within the one hundred (100) foot setback area may be developed as a public plaza to a level of design accepted by the Director of Planning and Community Development.~~

~~—For corner lots on International Boulevard, the parking structure must be faced with other uses as specified in subsection (A)(2) of this section on all sides adjacent to public and/or private streets for a distance of one hundred (100) feet from International Boulevard.~~

~~B.— Parking Structures on Properties Adjacent to All Other City Center Public and/or Private Streets. Parking structures shall be located within the maximum front yard setback, as specified in SMC 15.35.310, or built to the side or rear of the subject property when located behind or to the side of additional buildings on site. (Ord. 99-1050 § 8)~~



~~15.35.350 Projects on or Near the Edge of a UL or UM Residential Zone~~

~~Careful siting and design treatment is necessary to achieve a compatible transition between two (2) zones of differing height, bulk and scale requirements. In order to mitigate potential impacts of CB-C and UH zone development on neighboring residential districts, the following standards shall apply:~~

~~A.— Properties abutting a UL zone shall incorporate the following:~~

~~1.— A maximum building height of thirty-five (35) feet, relative to the base elevation of the adjacent UL zoned parcel(s) where that base elevation is higher than the base elevation of the proposed project, shall apply to all portions of a structure within sixty (60) feet of a UL zone, including access roadway widths; provided, that the overall height of any structure shall not exceed the maximum structure height specified in SMC 15.13.010; and~~

~~C.— Parking shall not be permitted within the side and/or rear yard building setback adjacent to a UL zone. (Ord. 99-1050 § 8)~~

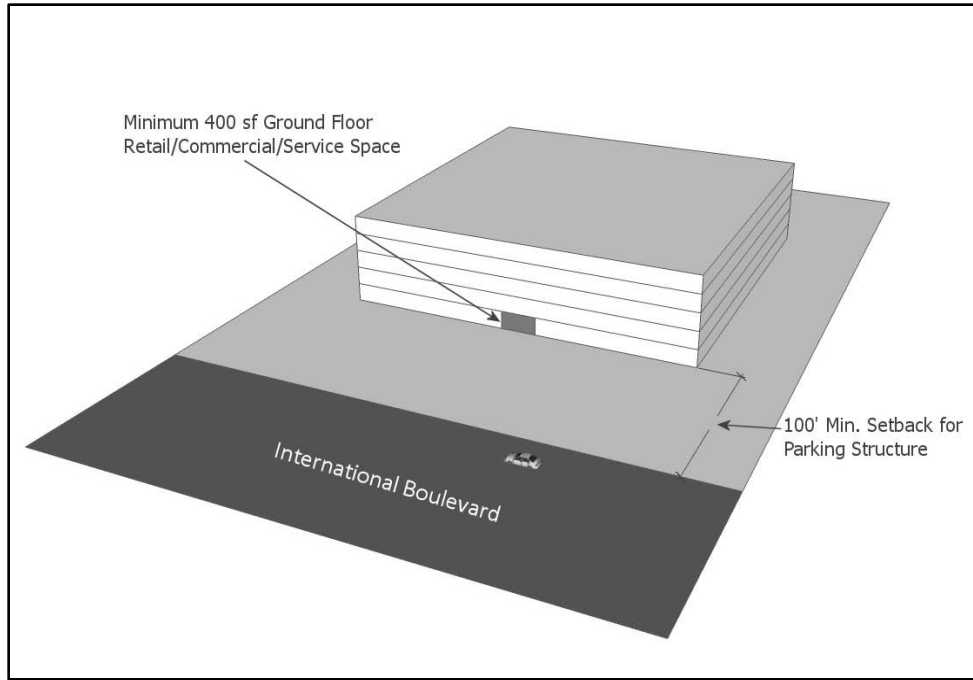


Figure 15.35.1033A – Building Type 1

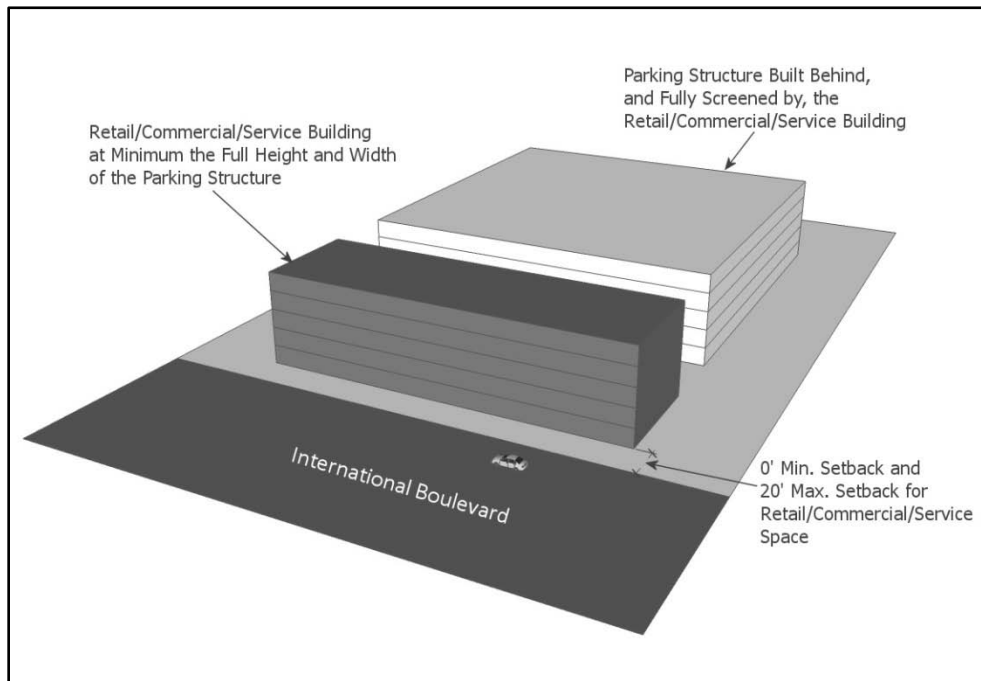


Figure 15.35.1033B – Building Type 2

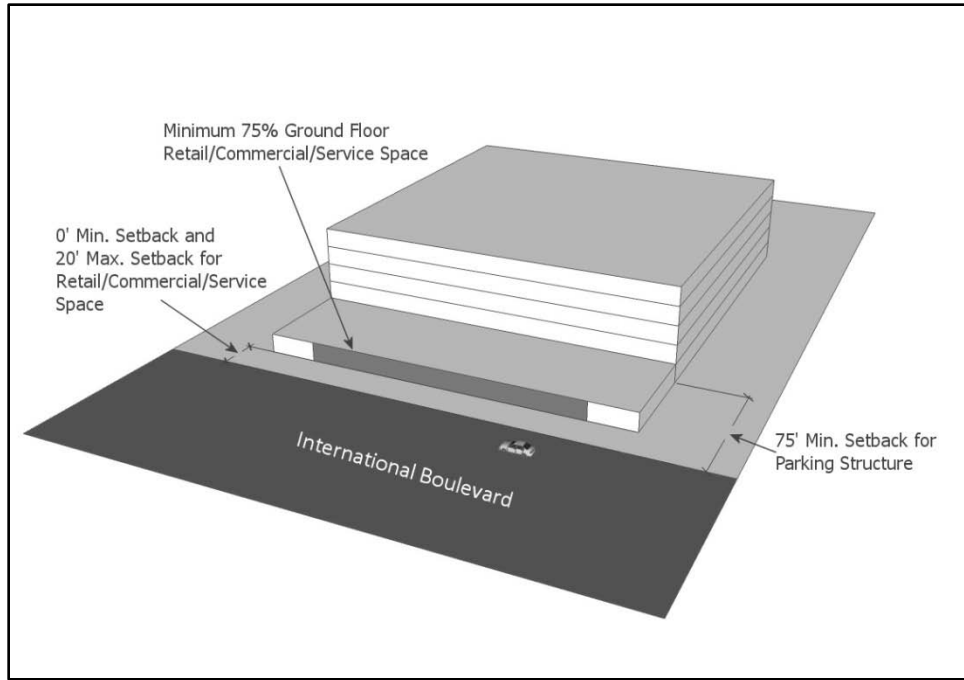


Figure 15.35.1033C – Building Type 3

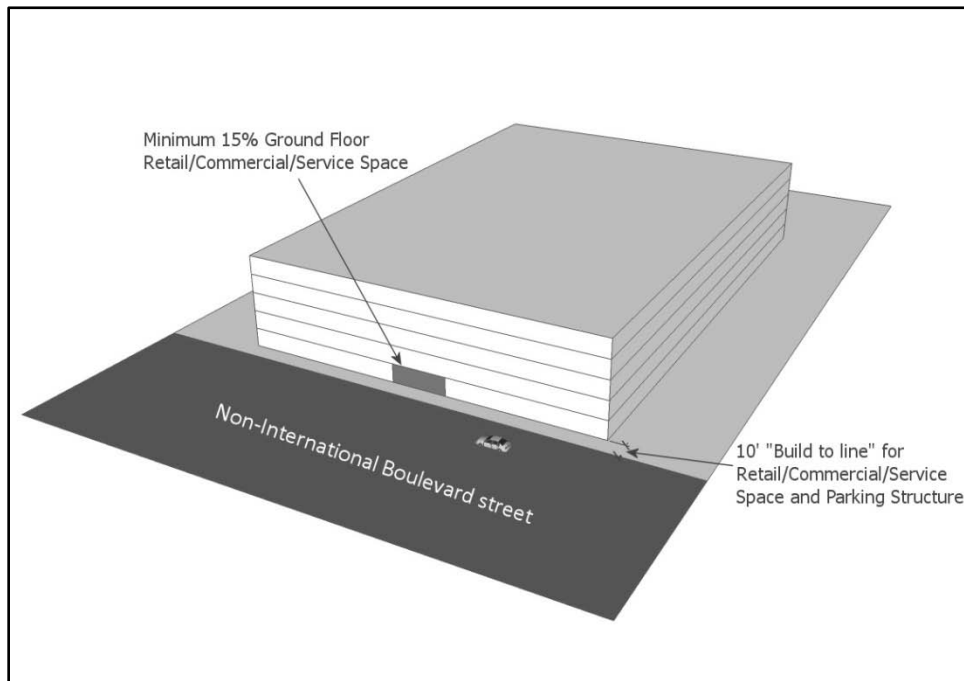


Figure 15.35.1033D – Building Type 4

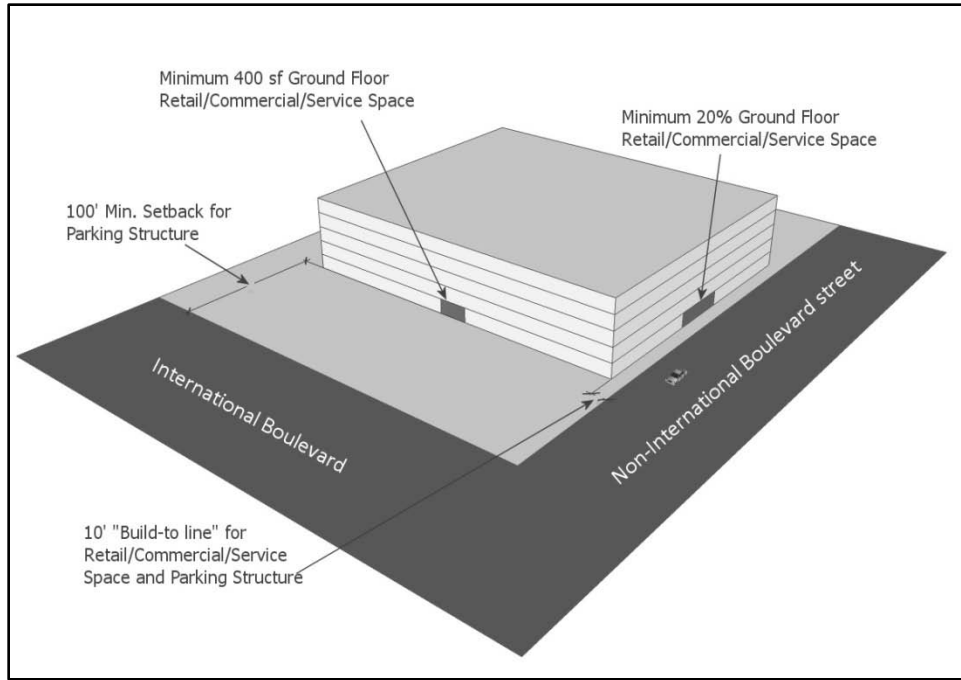


Figure 15.35.1033E – Building Type 5

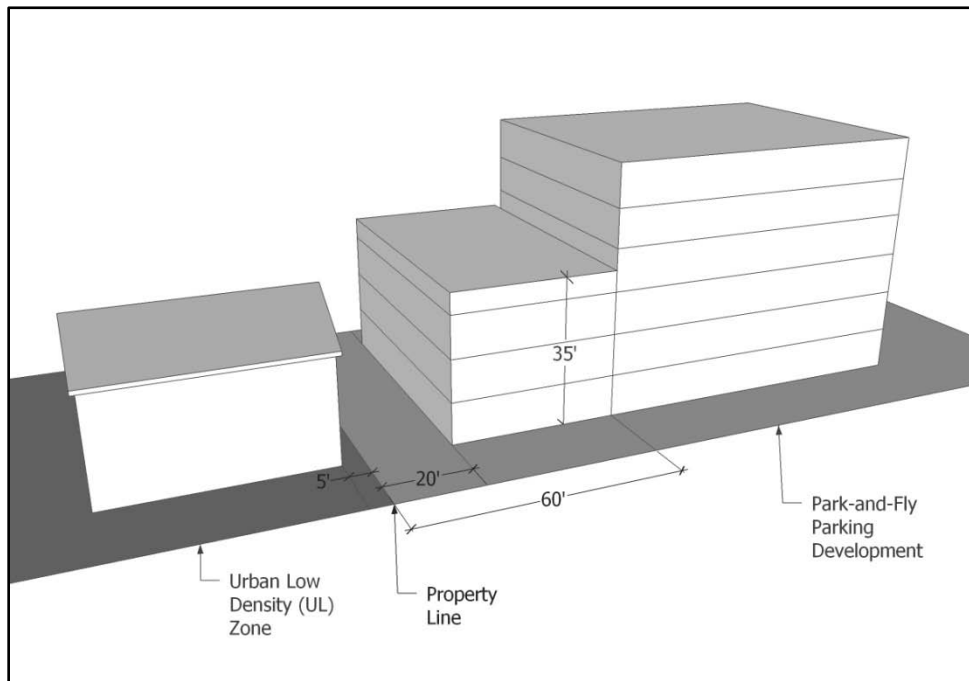
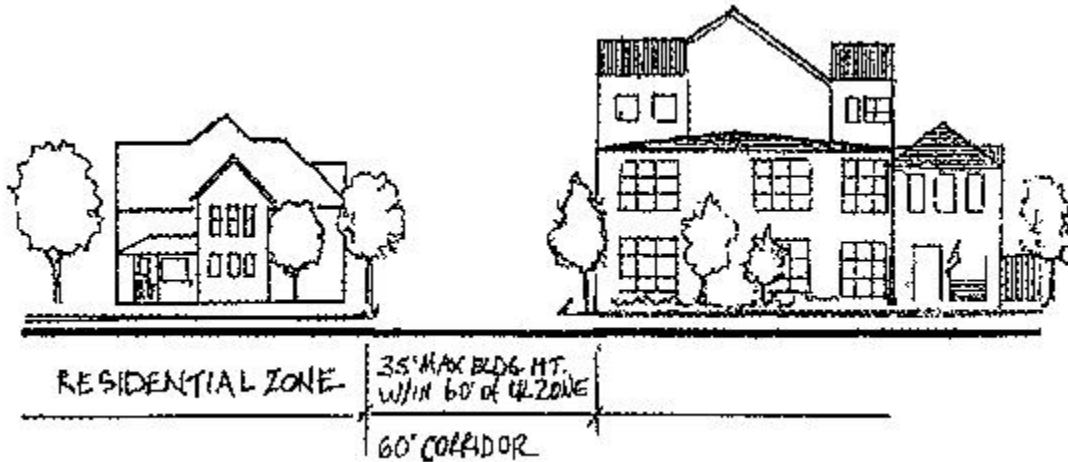


Figure 15.35.1033F – Minimum setback and maximum building height for developments adjacent to Urban Low Density (UL) zoned properties.



15.35.1040300 Site Design Planning and Building Orientation

Purpose: Developments Design structures to shall have both an external orientation, to the streetscape, and an internal orientation, to the pedestrian environment, with unifying open space and pedestrian pathways. Design emphasis should be given to the pedestrian, rather than the auto environment, through placement of parking in a less prominent location (such as underground, or to the rear of the building, rather than in front), the requirement of pedestrian level retail space; treatment of blank walls and facades and incorporation of prominent architectural features. Site layout should emphasize coordination of open spaces and pedestrian access with adjacent development or public places and compatibility with adjacent development with regard to scale, proximity and landscaping. Lighting and landscaping should allow for safety and visibility of public and semi-public areas. Dense development patterns shall be broken up with passive or active open spaces such as plazas, parks, trails and other means and linked wherever possible. Open spaces should be useable, have good access and take advantage of local amenities such as Bow Lake. Lighting and landscaping should allow for safety and visibility of public and semi-public areas.

(Ord. 99-1050 § 8)

Purpose: Break up dense development patterns with passive or active open spaces such as plazas, parks, trails and other means and link them wherever possible. Open spaces should be useable, have good access and take advantage of local amenities such as Bow Lake. (Ord. 99-1050 § 8)

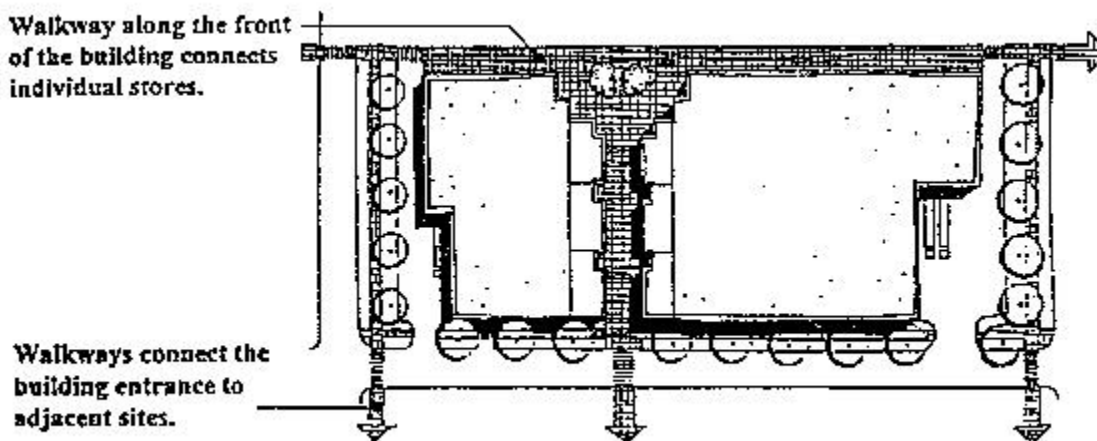
15.35.330 Relation to Adjacent Development

Proposed developments shall coordinate with current site planning and development efforts on adjoining parcels to take advantage of opportunities to mutually improve development design.

~~A. Adjacent developments shall link open spaces and landscaping whenever possible.~~

~~B. Proposed developments shall provide publicly accessible pedestrian connections to adjacent residential neighborhoods wherever possible, via a through block walkway or links to sidewalks. Provide stairs or ramps where necessary when topographic barriers, such as steep slopes, inhibit direct access to surrounding development or destination points, such as transit stops.~~

~~C. Where multi-family residential development is located adjacent to retail, commercial, employment, or institutional uses, side and/or rear yard landscape buffers shall be intersected by approved pedestrian circulation routes in order to facilitate convenient walking connections to adjacent uses or services.~~



15.35.1041320 Building Orientation Development Abutting Two (2) or More Street Frontages

A. Commercial park-and-fly parking developments shall be oriented as follows:

1. Buildings on lots with only one street frontage shall be oriented to said street.

2A. Buildings on corner lots shall orient front facades to both the corner and adjacent public and/or private street fronts. Pedestrian entries near or on the corner are encouraged.

3B. If the subject property abuts public and/or private streets classified as principal arterials by the SeaTac Comprehensive Plan along both its front and rear property lines, then the property owner shall either:

a1. Design a single building with facade entries oriented toward both the front and rear property lines; or

- b2. __Orient one (1) or more buildings toward the front property line along with a designated location for a current or future building or buildings oriented toward the rear property line.
(Ord. 08-1042 § 5; Ord. 99-1050 § 8)

- B. Building orientation shall include, at minimum, the primary pedestrian entrance facing the appropriate street, courtyard, or other common open space, with a pedestrian pathway connected to the sidewalk system.

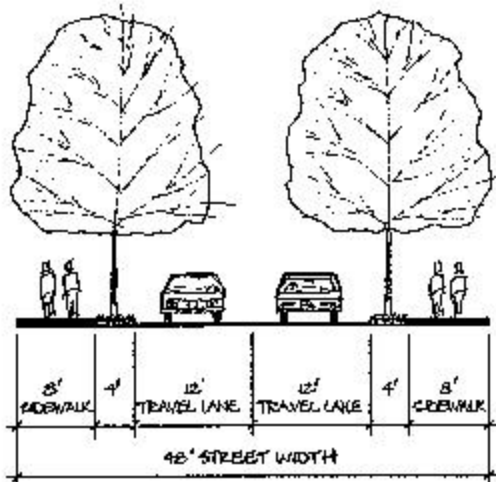
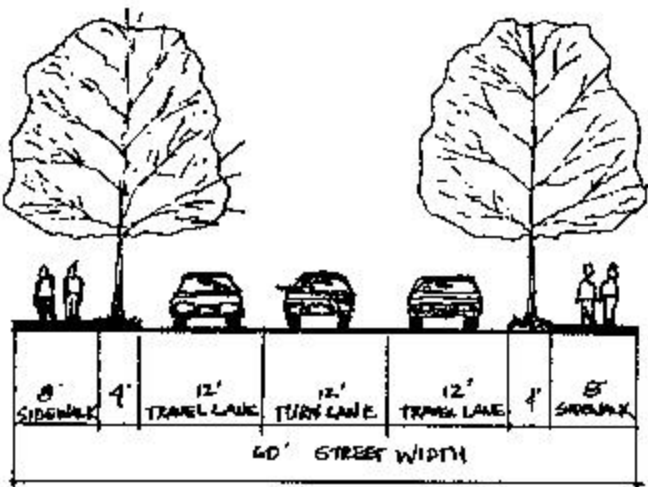
15.35.1042210 Vehicular Circulation Requirements

Commercial park-and-fly parking developments shall conform with the standards set forth in SMC Title 11, Streets, Sidewalks, and Public Thoroughfares.

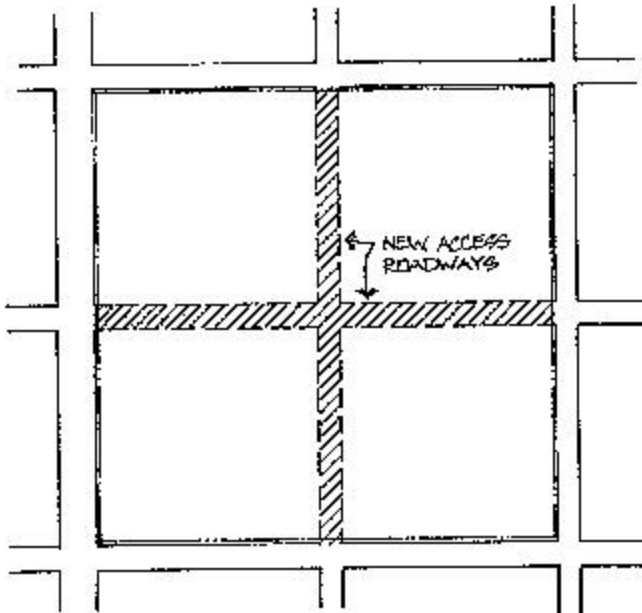
- ~~A. All new City Center streets shall be constructed within a minimum forty-eight (48) foot wide corridor (including streetfront pedestrian zones), and shall generally conform to the adopted City Center plan. Pedestrian and vehicular circulation within the City Center is intended to provide for public access, safe traffic flow, and connections to established vehicular and pedestrian routes, and is not intended to be applied prescriptively. Vehicular circulation shall be as approved by both the Director of Planning and Community Development and the Director of Public Works.~~

~~1. All collector streets shall be a minimum of forty-eight (48) feet in width and meet all applicable City Department of Public Works specifications.~~

~~2. All minor arterials shall be a minimum of sixty (60) feet in width and meet all City Department of Public Works specifications.~~



B.—All streets shall be designed to create blocks which are no greater than four hundred (400) feet on a side. In cases where topographic or other environmental constraints preclude the creation of a four hundred (400) feet by four hundred (400) feet foot block size, the four hundred (400) foot maximum block length shall apply to only two (2) sides of a block, and the maximum block length may be waived by the Director of Planning and Community Development.



~~C.— An owner or developer shall coordinate with owners of adjacent parcels and consolidate, wherever possible, vehicular circulation routes to interconnect public and/or private streets in conformance with the adopted City Center Plan. Where appropriate, circulation corridors shall extend to the boundary line of the site parcel in order to provide for future development of adjacent parcels and connections with existing public and/or private streets.~~

~~D.— Dead-end streets shall be permitted only where there is no feasible connection with an adjacent public and/or private street.~~

~~E.— Half-streets shall not be allowed, except as an interim circulation route as approved by both the Director of Planning and Community Development and the Director of Public Works, in which the other half of the public or private street shall be developed on an adjacent parcel.~~

~~F.— Public and private streets should not be enclosed. In cases where buildings are allowed to span public or private streets, the following standards shall apply:~~

~~1.— The minimum ceiling height shall meet highway standards;~~

~~2.— Lighting sufficient to provide a safe pedestrian environment shall be in operation at all times;~~

~~3.— At the time of development, ground level retail space shall be constructed in accordance with the provisions of SMC 15.35.620, except as provided below:~~

~~a.— Ground level retail space shall be constructed either on one (1) side of City Center streets (collectors or minor arterials), for the full length of the enclosed area; or~~

~~b.— Shall be constructed on both sides of the collector or minor arterial for fifty percent (50%) of the length of the enclosed area; and~~

~~c.— Shall be served by the minimum utilities necessary for occupancy. These utilities include electricity, and sewer and water service;~~

~~4.— Ground level retail space shall conform to the ground level transparency requirements specified in SMC 15.35.510;~~

~~5.— A minimum of one (1) piece of public art, approved by the Director of Planning and Community Development, shall be included for every fifty (50) feet of enclosed length. Art work may include, but is not limited to the following suggestions:~~

~~a.— Artistic wall treatments such as painted murals, bas-relief murals, photographic montages, mosaics;~~

~~b.— Artistic pavement or ceiling treatments as approved by the Director of Planning and Community Development;~~

~~c.— Neon lighting sculptures or other artistic lighting displays;~~

~~6.— Ventilation systems to provide air inside the covered area that is at least as clean as ambient levels in the City Center outside the enclosure shall be in operation at all times;~~

~~7.— The interior space of the covered area shall appear in all aspects to be “pedestrian oriented.” (Ord. 99-1050 § 8)~~

15.35.1043220 Pedestrian Circulation Requirements

~~Purpose: Sufficient vehicular circulation should be provided through the establishment of an adequate network of collectors and minor arterials. Pedestrian corridors should be inviting in their overall design, such as through the provision of street furniture and landscaping, and should feel secure by providing adequate safety measures, such as lighting. (Ord. 99-1050 § 8)~~

- A. ~~All site plans proposing multiple buildings designed for residential occupancy or business access~~ shall connect building entrances to one another and to pedestrian ways on adjacent public and/or private streets via a minimum four (4) foot wide walkway system separated from vehicular traffic. Public sidewalks may be considered part of the walkway system if they provide convenient movement between structures. Fences, landscaping and other site improvements shall be located so as not to impede safe and convenient pedestrian circulation.
- B. ~~On-site pedestrian ways shall be designed to connect to off-site pedestrian way systems on adjoining properties and public and/or private streets. On-site extensions of pedestrian circulation systems shall align with existing pedestrian off-site links.~~

- C. Pedestrian-only corridors separate from the vehicular street system, as specified by the adopted City Center Plan (see “Pedestrian Connections” in City Center Plan, Figure 5.1), shall be a minimum of twenty (20) feet wide with a minimum twelve (12) foot pathway of an approved surfacing material.

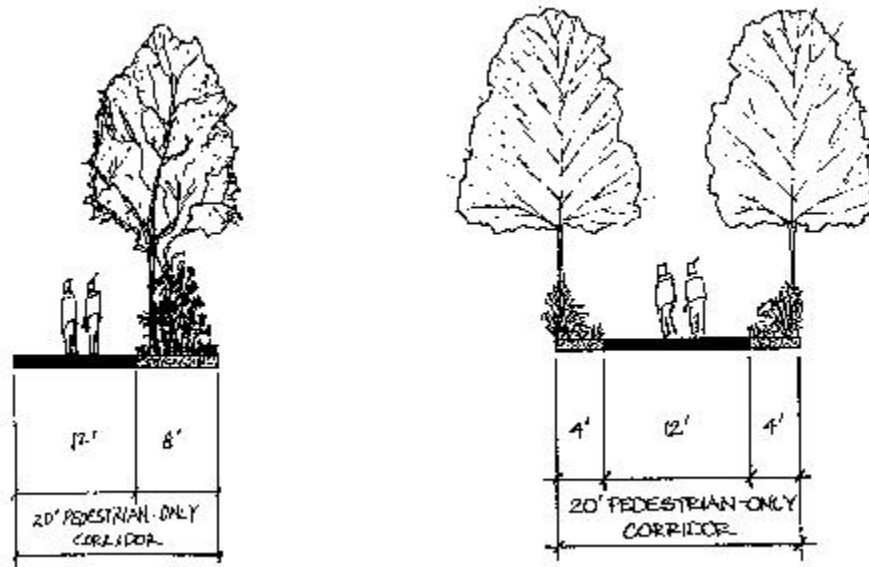
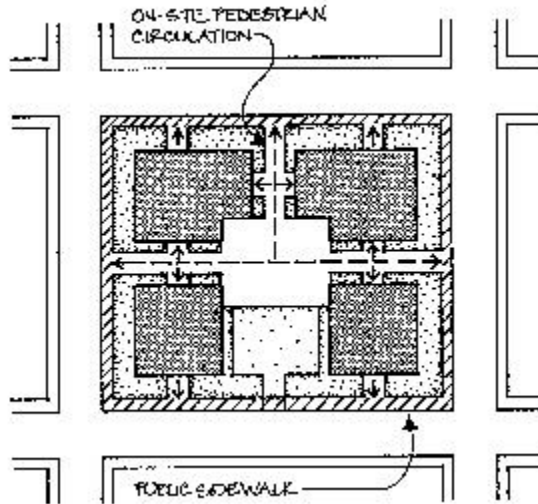


Figure 15.35.1043A – Minimum dimensions of pedestrian-only corridors.

- D. Buildings or structures approved by the Director of **Planning and** Community **and Economic** Development to be built across a designated pedestrian-only corridor, as specified in the City Center Plan (see “Pedestrian Connections” in City Center Plan, Figure 5.1), shall provide public pedestrian access through said structures at least between the hours of 6:00 a.m. and 8:00 p.m. daily.
- E. Public easements for pedestrian circulation shall be open to the public twenty-four (24) hours a day, except as specified in subsection (D) of this section. Private easements for pedestrian circulation should remain open to the maximum extent possible.
- F. Primary pedestrian circulation and access shall be at grade. Elevated pedestrian walkways, if approved by the Director of **Planning and** Community **and Economic** Development, may be permitted for the following purposes:
1. To provide an extension to or direct connection with an elevated walkway/moving sidewalk;
 2. To overcome obstacles of terrain;
 3. To connect immediately adjacent components of a single development; or
 4. To connect with elevated transit stations.

- G. To promote public transit use, paved sidewalks or walkways shall be provided between building entrances and the nearest transit stop located within or adjacent to the subject property. Wherever possible, buildings shall be sited adjacent to or connected with transit stop facilities. Lighting shall be provided along pedestrian walkway connections and adjacent to transit stop facilities. (Ord. 99-1050 § 8)



15.35.1044 ~~15.35.400~~ Open Space and Amenities
15.35.410 ~~Minimum Open Space Area Required~~

A. Minimum Open Space Required

- A1. _____ A minimum of ~~fiveten~~ percent (~~510~~%) of net site area, excluding portions of a parcel classified as wetland; storm water facility, provided that such storm water facilities are at grade and not covered; or open water, shall be set aside as usable outdoor open space accessible to the public.
- a1. Required landscaping and sensitive area buffers without common access links, such as pedestrian trails, shall not be included toward meeting the minimum open space area requirement.
- b2. _____ Driveways, parking, or other auto uses shall not be included in any usable outdoor open space area.
- c3. Areas of a parcel with slopes greater than eight percent (8%) shall not qualify as usable outdoor open space, unless the area has been developed with an enhanced accessibility system of stairs, ramps, terraces, trails, seating areas, or other site improvements as approved by the Director of ~~Planning and~~ Community and Economic Development.
- d4. _____ Parcels adjacent to Bow Lake may not count areas of open water for the purpose of calculating the open space area requirement.

- e5. Wetland buffer and setback areas shall also be excluded for the purpose of calculating the open space requirement.

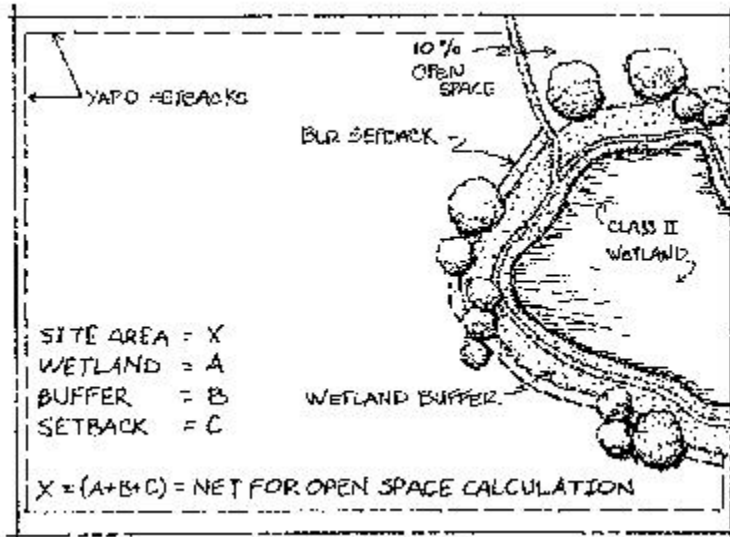


Figure 15.35.1044A – Open space calculation when sensitive areas are present.

2B. _____ Usable open space shall include one (1) or more of the following:

- a1. Active outdoor recreation areas;
- b2. ___ Multi-purpose green spaces;
- c3. Pedestrian-only corridors separate from the public or private roadway system and dedicated to passive recreation, including access links in sensitive area buffers. The square footage (length times width) of pedestrian-only corridor shall be counted as usable open space; and/or
- d4. ___ Publicly accessible plazas, courtyards, pocket parks and decorative paving areas constructed contiguous with a new or existing sidewalks located either within the front yard setback or elsewhere on-site. Developments proposing on-site plazas and pocket parks as publicly accessible project amenities shall link the open space elements with adjacent sidewalks, pedestrian paths, and/or bikeways. Publicly accessible courtyard designs shall conform to the following standards:
- ia. ___ The courtyard dimension is a measurement of the usable open space between two (2) buildings or to a property line, and shall have a width equal to the height of the building, up to a maximum of seventy-five (75) feet, but in no cases less than twenty (20) feet.
- ii b. ___ If the enclosing walls of a courtyard terrace upward and back with succeeding stories, the courtyard dimension shall be measured from the lowest enclosing floor or projection.

3C. _____ The front yard open space requirement as per SMC 15.35.~~1044(C)420~~, and lakefront open space requirement as per SMC 15.35.~~1044(D)430~~ may be counted toward the minimum open space area requirement.

a1. Developments have the option of contributing to a City Center open space fund in lieu of setting aside additional on-site open space area greater than the minimum required in both the front yard, as per SMC 15.35.~~1044(C)420~~, and along lakefronts, as per SMC 15.35.~~1044(D)430~~. The City shall use the funds contributed to the City Center open space fund within six (6) years on an approved open space/park project or return said funds to contributors. Revenue from the City Center open space fund may be applied only to open space/park projects within the defined City Center area.

b2. ___ To receive exemption for an amount of on-site open space totaling less than the required open space area, a contribution to the City Center open space fund shall be made in an amount that equals the monetary value of that portion of site area which is less than the required open space.

4D. _____ Usable open space areas on-site shall be organized and designed in a manner that allows for maximum integration with open space on adjacent parcels; ~~as specified in SMC 15.35.330~~. (Ord. 99-1050 § 8)

B. Location of Open Space

Open space shall be located at the front of a property or at the side of the property provided it is contiguous to the required front yard open space as described below. Exceptions shall be made for open space incorporating Bow Lake as a focal point as described in SMC 15.35.1044(D).

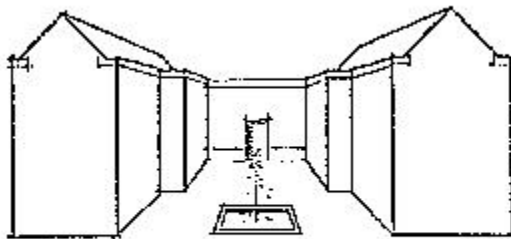
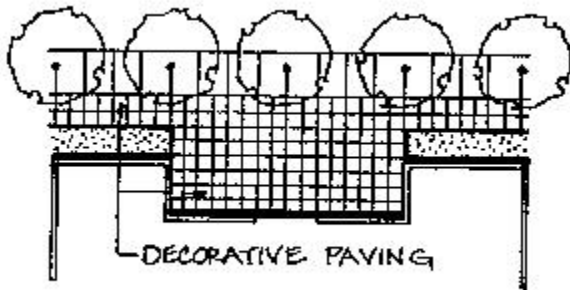
C.~~15.35.420~~ _____ Front Yard Open Space

The following front yard open space regulation shall supersede the street frontage landscape requirement as specified in SMC 15.14.020(C) and 15.14.060. The building facade landscaping requirement shall continue to apply to uses in the City Center, except under pedestrian weather protection structures, as specified in SMC 15.35.~~1045, Landscaping570~~.

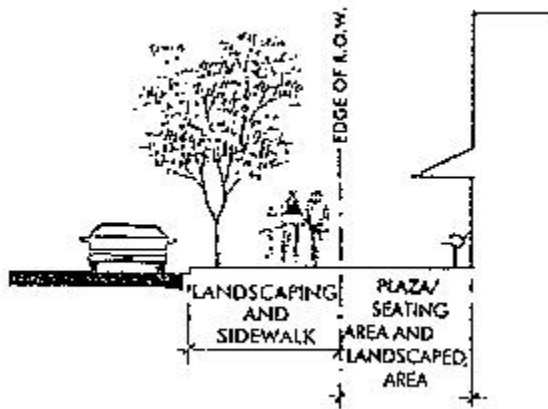
1A. _____ Front yard open space area equal to the square footage of a five (5) foot strip along the length of the street-facing front facade(s) shall be developed and arranged in a manner that is accessible to the public at all times, directly connected to a sidewalk or pedestrian pathway, and bordered on at least one (1) side by, or readily accessible from, approved structure(s) on-site. Front yard open space shall be placed in one (1) or more of the following ways, as approved by the Director of ~~Planning and~~ Community and Economic Development:

- a1. Plaza, Courtyard, or Pocket Park. Publicly accessible open space of a minimum two hundred (200) square feet that is adjacent to a pedestrian building entrance and consisting of at least fifty percent (50%) decorative paving. The remaining percentage of required open space area may be installed as plantings within or immediately adjacent to the plaza, courtyard, or pocket park. One (1) tree shall be required for every two hundred (200) square feet of decorative paving area. Decorative paving areas shall be constructed of such materials as stamped, broom finish, or scored concrete; brick or modular pavers;
 - b2. Multi-Purpose Green Space. A combination of grass, pedestrian ways, and seating areas of a minimum two hundred (200) square feet. One (1) tree shall be required for every two hundred (200) square feet of green space area; and/or
 - c3. Decorative Paving Contiguous with Sidewalk. A minimum five (5) foot wide decorative paving area constructed contiguous with a new or existing sidewalk along the length of the front yard building facade, coupled with a direct connection between the building entrance and sidewalk.
- 2B. Outdoor Seating. Publicly accessible plazas, courtyards, and pocket parks shall include at least one (1) linear foot of seating per each forty (40) square feet of plaza, courtyard, or pocket park space on-site. Outdoor seating shall be in the form of:
- a1. Freestanding outdoor benches of a minimum sixteen (16) inches wide; or
 - b2. Seating incorporated into low walls, raised planters or building foundations at least twelve (12) inches wide and eighteen (18) inches high.
- 3C. Focal Point For Plazas, Courtyards and Pocket Parks. In addition to seating, publicly accessible plazas, courtyards, and pocket parks should incorporate one (1) or more of the following open space amenities in order to encourage pedestrian use and activity:
- a1. Public art, such as a water feature or sculpture;
 - b2. Transit stops;
 - c3. Performance/stage areas; or
 - d4. Other public amenities, as approved by the Director of ~~Planning and~~ Community and Economic Development.
- 4D. —Accessory Site Furnishings:
- a. Accessory site furnishings shall be located so as not to obstruct pedestrian access along sidewalks and to businesses.

- b4. Waste receptacles, movable planters and other accessory site furnishings shall be of a design which is compatible with the design of the plaza, courtyard, or pocket park, through the use of similar detailing or materials. (Ord. 99-1050 § 8)



Organized around an outdoor space.

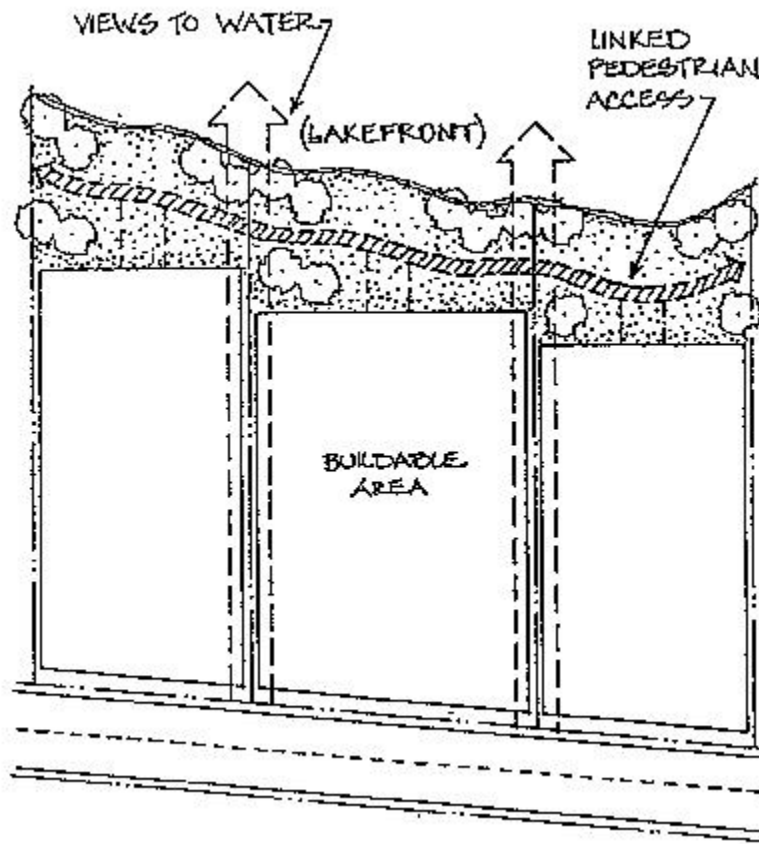


[Figure 15.35.1044B – Examples of front yard open space elements.](#)

D.15.35.430 Incorporating Bow Lake as a Focal Point

New development and major redevelopment adjacent to Bow Lake shall be designed to increase opportunities for the public to both view and access the lakefront area, while at the same time minimizing impacts on the waterway’s biologic and hydrologic functions.

- 1A. _____ The Bow Lake waterfront buffer and building setback area, as required in SMC 15.30.190, should be made publicly accessible through the construction of pedestrian access trail links, seating areas, and shoreline viewing points, in conformance with SMC 15.30.300.
- 2B. _____ Adjacent lakefront developments shall link waterfront open spaces and associated pedestrian circulation systems.
- 3C. _____ Properties adjacent to Bow Lake shall provide a pedestrian access corridor to the lakefront area from an adjacent public or private street. Side yard pedestrian access links may be shared between adjacent properties.
- 4I. _____ All nonresidential properties abutting Bow Lake and associated wetlands shall provide at least one (1) clearly marked public entry facing the lake designed to connect with the planned pedestrian trail. (Ord. 99-1050 § 8)



[Figure 15.35.1044C – Incorporating Bow Lake into open space design.](#)

15.35.1045 Landscaping

Landscaping for commercial park-and-fly parking developments shall conform to the regulations listed in SMC 15.14 with the following exceptions:

~~15.35.930~~A. For portions of parking structures without a ground floor retail/commercial/service use, the following building facade landscaping is required:

1. A five (5) foot wide building facade landscape strip is required for park-and-fly parking developments and consistshall consisting of the following:
 - 1a. A mix of evergreen shrub groupings spaced no more than four (4) feet apart that do not exceed a height of six (6) feet at maturity;
 - 2b. Ground cover; and
 - 3e. Seasonal displays of flowering annual bedding plants.
2. ~~15.35.570~~C. Building facade landscaping shall not be required under pedestrian weather protection structures along public or private street frontages. Any facade landscaping provided under pedestrian weather protection structures shall be of such width that a minimum four (4) feet of unobstructed walking area remains under the building awning, canopy, overhang, or other weather protection structure.

~~D.— Building facade landscaping in front of a ground floor retail use shall be designed and maintained to avoid obscuring visibility of street-facing windows or limiting access to building entrances, and shall consist of:~~

- ~~1.— Evergreen shrubs spaced no more than four (4) feet apart that do not exceed a height of four (4) feet at maturity;~~
- ~~2.— Ground cover; and~~
- ~~3.— Seasonal displays of flowering annual bedding plants.
(Ord. 99-1050 § 8)~~

15.35.1046335 The Layout and Width of Streetfront Pedestrian Zone

Within the City Center, all new sidewalks and street improvements shall include a streetfront pedestrian zone, consisting of street landscaping and a sidewalk clear-through zone.

- A. Street Landscaping. A minimum four (4) foot wide street landscaping zone shall be required adjacent to the street curb, consisting of a combination of trees, landscaping, light poles, and street furniture in a manner to be approved by the Director of ~~Planning and Community~~ and Economic Development.

1. The street landscaping zone will include either tree wells and grates for street trees; or shrubs, ground cover and/or lawn in addition to street trees.
 2. Street trees shall be deciduous shade trees capable of at least twenty-five (25) feet in height. Street trees shall be planted within the street landscaping zone along public and/or private streets and be spaced no more than thirty (30) feet apart as described in SMC 15.14.130, except where variations in tree spacing, as approved by the Director of ~~Planning and~~ Community and Economic Development, may be considered to enhance plaza areas, emphasize building focal points or avoid visually blocking retail storefront entrances.
- B. Sidewalk Clear-Through Zone. A pedestrian sidewalk clear-through zone shall be created along the public and/or private street frontage consisting of a minimum eight (8) foot wide paving area free of physical obstructions to pedestrian movement.
1. The combination of street landscaping and sidewalk clear-through zones shall form a minimum twelve (12) foot wide pedestrian zone between the street curb and any building edge or facade landscaping bed.
- C. Fences over four (4) feet in height or other features that form continuous visual barriers or block views to the windows of a ground level retail/commercial or service use are prohibited within the front yard setback zone.
- D. Monument signs shall be located according to an approved site plan and in a manner that does not obstruct pedestrian movement. (Ord. 99-1050 § 8)

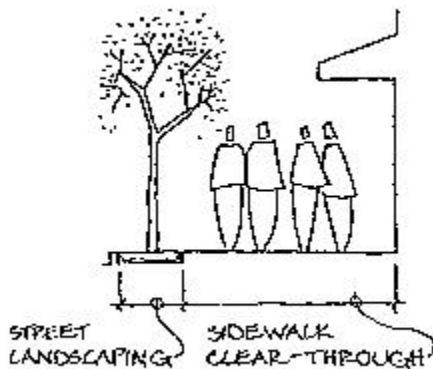


Figure 15.35.1046A – Sidewalk clear-through zone.

15.35.1047340 Driveway Entrances

- A. Driveways serving front yard porte cochere building entries shall be approved by the Director of ~~Planning and~~ Community and Economic Development and include only the short-term parking that can be accommodated along one (1) double-loaded drive aisle.
- B. Pedestrian entry routes interrupted by driveways shall be distinguished from the driveway surface by decorative paving to the building entrance.

- C. Driveway stacking lanes associated with drive-through facilities shall not be located between the building and any property line adjacent to a public and/or private street. (Ord. 99-1050 § 8)

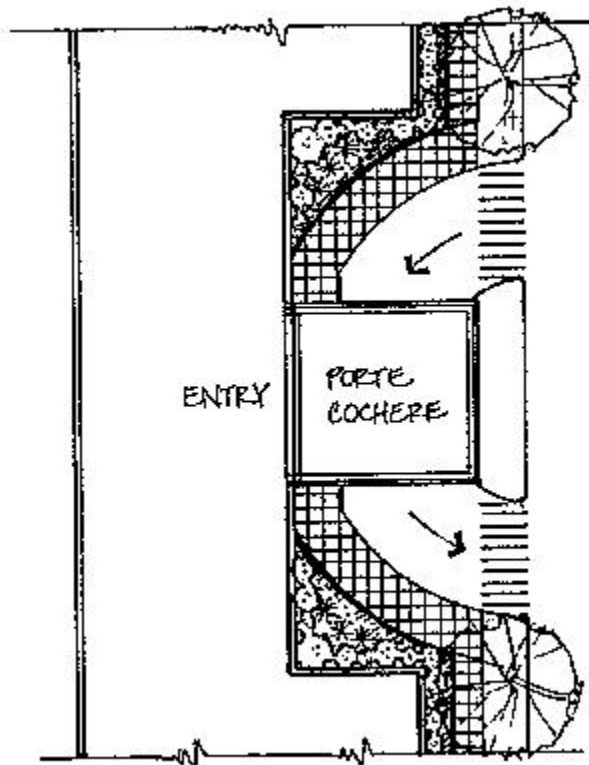
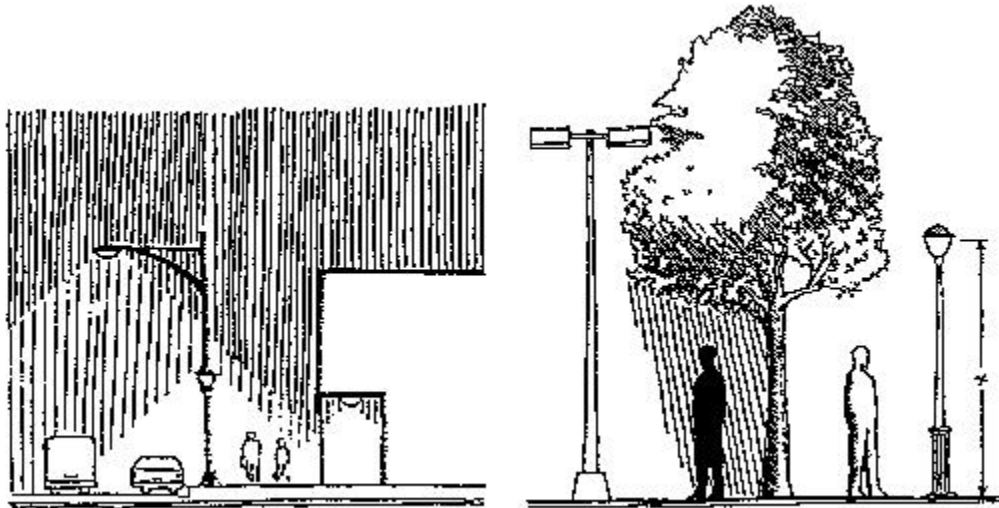


Figure 15.35.1047A – Driveway serving a porte cochere.

15.35.345 Exterior Lighting

- A. ~~Lighting standards shall be no greater than sixteen (16) feet in height, and used to illuminate surfaces intended for pedestrians or vehicles, as well as building entries. Light fixtures shall be sited and directed to minimize glare around residences.~~
- B. ~~Exterior lighting shall be used to identify and distinguish the pedestrian walkway network from car or transit circulation. Along pedestrian circulation corridors, lighting standards shall be placed between pedestrian ways and public and/or private streets, driveways or parking areas. The level of lighting shall conform with the requirements of Chapter 17.40 SMC, Walkway, Bikeway and Park Lighting.~~

~~C. Light standard designs shall be approved by the Director of Planning and Community and Economic Development, and be in conformance with a consistent lighting standard design throughout the City Center area. (Ord. 03-1033 § 9; Ord. 99-1050 § 8)~~



15.35.1050500 Building Design

Purpose: Buildings ~~shall~~ should be designed to promote an architecturally appealing environment. Design emphasis should be given to the pedestrian through the provision of inviting building entries, street-level amenities and other structural and facade elements to encourage pedestrian interaction. ~~Purpose: Design Commercial park-and-fly structures or structures associated with commercial park-and-fly development~~ ~~Parking structures shall be designed to blend in with adjacent development. Emphasize design features that~~ minimize the obtrusiveness of the parking use and encourage architectural compatibility with adjacent development. (Ord. 99-1050 § 8)

15.35.900 Parking Structures

15.35.1051 All Structures

~~This subsection shall apply to all structures associated with a commercial park-and-fly parking development.~~

A. ~~15.35.920 Parking Structure~~ Character and Massing

14. Architectural Focal Points

a. All building facades visible from public view shall contain a focal point near the main activity areas, building entrances, building corners, or pedestrian plazas using at least one (1) of the following methods:

i. Unique materials;

ii. Tower elements and/or rooflines that accentuate the corner;

iii. Height differentiation; and/or

iv. Variation in building massing.

~~D. Buildings or structures that terminate view corridors shall include architectural features that increase the visibility and landmark status of the subject building facade, such as a clearly defined building modulation, pedestrian entry feature, and/or roof line that accentuates the building as a focal point. (Ord. 99-1050 § 8)~~

b. Developments on at the intersection of a principal arterial with either a principal or minor arterial International Boulevard corner lots shall contain ~~a~~include architectural focal points that increases the visibility and landmark status of ~~of corner~~the building(s) using at least, ~~such as~~ one (1) of the ~~or~~ more of the following following methods:

i. Orientation of the building;

ii. Unique materials;

iii. Architectural features;

~~iva. Transparent glazing incorporated into corner building design;~~

ivb. Tower elements and/or roof lines that accentuate the corner;

v. Height differentiation; and/or

vi. Variation in building massing.

~~e. Balconies or building terraces at or near the corner.~~

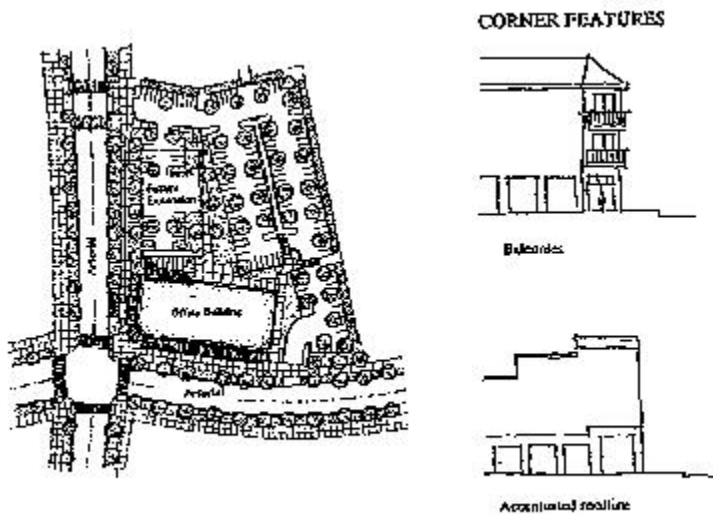


Figure 15.35.1051A – Corner lot development using unique architectural features and a tower element as a focal point. 15.35.560 Character and Massing

Building facades one hundred (100) feet or greater in length shall incorporate vertical and/or horizontal variations in setback, material or fenestration design along the length of the applicable facade, in at least two (2) of the following ways listed below (see SMC 15.35.920 for character and massing requirements specific to parking structures):

A. Vertical Facade Changes. Incorporate intervals of architectural variation at least every eighty (80) feet over the length of the applicable facade, such as:

1. Varying the arrangement, proportioning and/or design of windows and doors;

- ~~2. Incorporating changes in architectural materials; and/or~~
- ~~3. Projecting forward or recessing back portions or elements of the applicable facade.~~
- ~~a. Front facades incorporating a variation in building setback shall include within the setback such architectural elements as covered or recessed building entries, plazas or courtyards, storefront or bay windows, seating and/or planting areas.~~
- ~~B. Horizontal Facade Changes. Designed to differentiate the ground floor from upper floors, such as:~~

2. Building Modulation

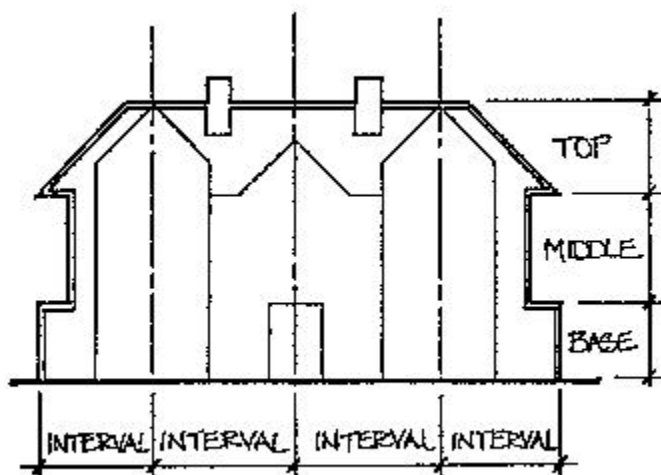
- ~~a. Vertical Modulation. Buildings~~~~Parking structure elevations over one hundred fifty (150) feet in length shall employ one (1) or more of the following methods of vertical modulation at least every forty (40) feet for street facing facades of any length and at least every eighty (80) feet for non-street facing facades greater than one hundred fifty (150) feet in length~~~~incorporate vertical and/or horizontal variation in setback, material or fenestration design along the length of the applicable facade, in at least one (1) or more of the following ways:~~
- ~~i~~
- ~~A. Vertical Facade Changes. Incorporate intervals of architectural variation at least every eighty (80) feet over the length of the applicable facade, such as:~~
- ~~hi. Varying the arrangement, proportioning and/or design of doors, windows, or garage floor openings;~~
- ~~ii2. Incorporating changes in architectural materials;~~
- ~~iii3. Projecting forward or recessing back portions or elements of the parking structure facade;~~
- ~~bB. Horizontal Modulation. Buildings shall employ one (1) or more of the following methods of hHorizontal modulation~~~~Facade Changes. Designed to differentiate the ground floor from upper floors, such as:~~
- ~~i1. Street facing facades of any length:~~
- ~~• Stepping back the upper floors from the ground floor parking structurebuilding facade;~~

- ~~ii2.~~ Changing materials between the ~~parking structure building~~ base and upper floors; and/or

- ~~iii3.~~ Another architectural element that accomplishes the intent of the horizontal modulation standard as approved by the Director of Community and Economic Development. Including a continuous cornice line or pedestrian weather protection element between the ground floor and upper floors. (Ord. 99-1050 § 8)

ii. Non-street facing facades greater than one hundred fifty (150) feet in length:

- ~~1.~~ Stepping back the upper floors from the ground floor parking structure facade;
- ~~2.~~ Changing materials between the parking structure base and upper floors; and/or
- ~~3.~~ Including a continuous cornice line or pedestrian weather protection element between the ground floor and upper floors. (Ord. 99-1050 § 8)



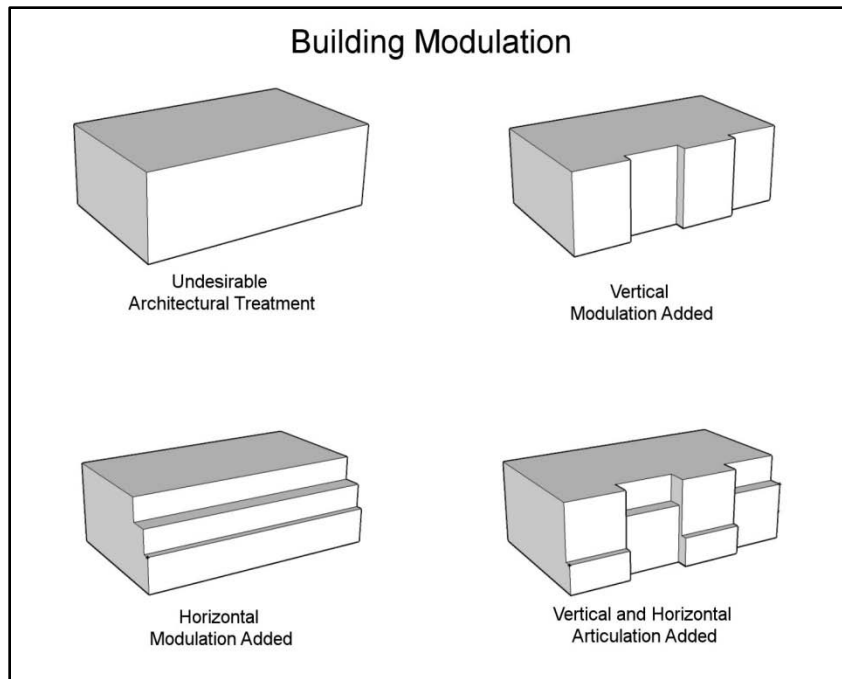


Figure 15.35.1051B – Examples of vertical and horizontal building modulation by projecting forward or recessing back portions or elements of the facade.

3. International Boulevard Façade Treatment

In order to enhance building design at the street level, the first two floors of building façade fronting on International Boulevard shall have the appearance of a retail/commercial/service building through the careful architectural integration and coordination of the design of the ground floor and upper floor including the following:

- a. First/Ground Floor: Development shall comply with ground floor use requirements in SMC 15.35.1032, Retail/Commercial/Service Spaces Requirements and Front Yard Setbacks Chart, and design requirements as listed in SMC 15.35 and elsewhere in the code, including but not limited to, transparency, weather protection and pedestrian entrances.
- b. Second Floor: One hundred percent (100%) of the building façade, up to a minimum of eighteen feet (18’), shall replicate the appearance of a retail/commercial/service building through utilizing a combination of the architectural treatments listed below:
 - i. Glazing, clerestory windows, architectural variation, high quality building materials or other unique high quality architectural treatments

to be approved by the Director of Community and Economic Development.

B. 15.35.520 Pedestrian **Building** Entries

- 1A. _____ Primary **pedestrianbuilding** entries shall be designed to be clearly visible or recognizable from an adjacent public or private street through the incorporation of two (2) or more of the following features:
- a1. Canopies, awnings, or other entry coverings that provide pedestrian shelter and interest;
 - b2. _____ Distinctive architectural elements such as a variation in the building footprint, roof form, or amount of transparent glazing;
 - c3. Pedestrian-scaled ornamental lighting no greater than sixteen (16) feet in height;
 - d4. _____ Landscaping designed as entry focal point.
- 2B. _____ All ground level **pedestrianbuilding** entries shall be located no more than three (3) feet above or below the grade of the sidewalk. In the case of an allowable grade difference between a building entry and adjoining sidewalk, provide stairs and/or ramps to accommodate pedestrian access. (Ord. 99-1050 § 8)



Figure 15.35.1051C – Pedestrian entry with a covering and a distinctive architectural element.

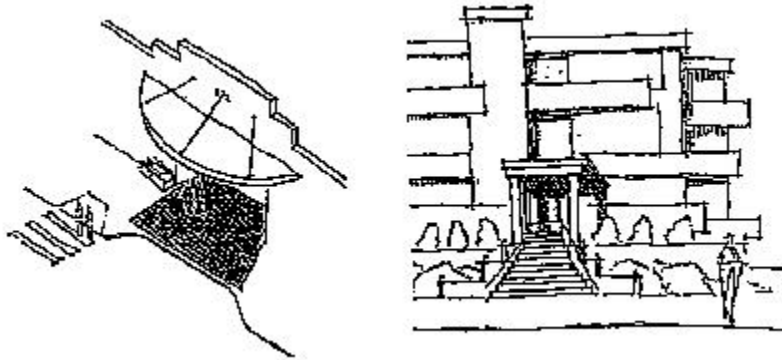


Figure 15.35.1051D – Pedestrian entry with a covering and pedestrian-scaled ornamental lighting.

C. 15.35.510 Ground Floor Transparency Requirements

1. Windows shall be provided on the street level rather than blank walls to encourage a visual link between the business and passing pedestrians.

22A. – Transparency requirements shall apply to the portions of buildings with a ground floor retail/commercial/-or service use adjacent to a street or pedestrian way, as defined in SMC 15.35.620 and in the City Center Use Charts (SMC 15.35.100 through 15.35.160), including portions of buildings where ground floor uses are convertible to a retail/commercial or service use. Transparency requirements shall not apply to the portions of a buildings with ground floor housing or parking areas.

- a1. Windows shall cover at least sixty percent (60%) of the wall area-public street facing ground floor building wall adjacent to a street or pedestrian way-area.
- b. At the first floor building level, dDarkly tinted, mirrored or reflective glass shall not be used. Lightly tinted windows are allowed for nonretail ground floor uses.
- cb2. Transparency requirements shall apply to that area of the ground floor building wall fronting the street up to the finished ceiling height of the first floor building space.
- de3. Windows shall begin twelve (12) to thirty (30) inches above the finished grade of the first floor building space. (Ord. 99-1050 § 8)

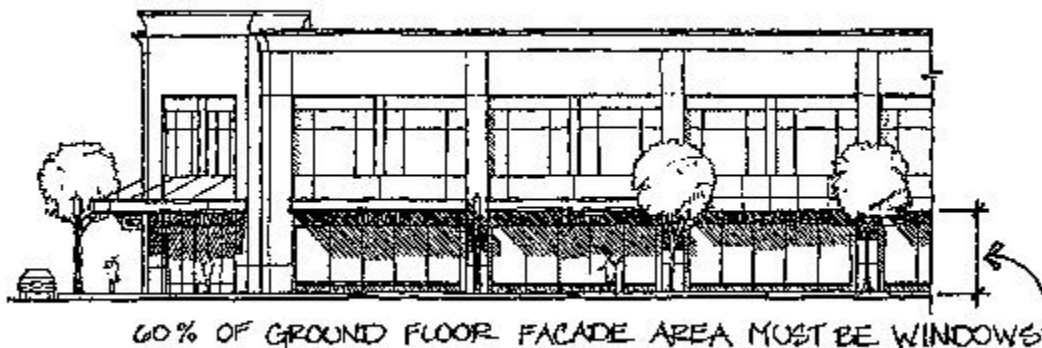


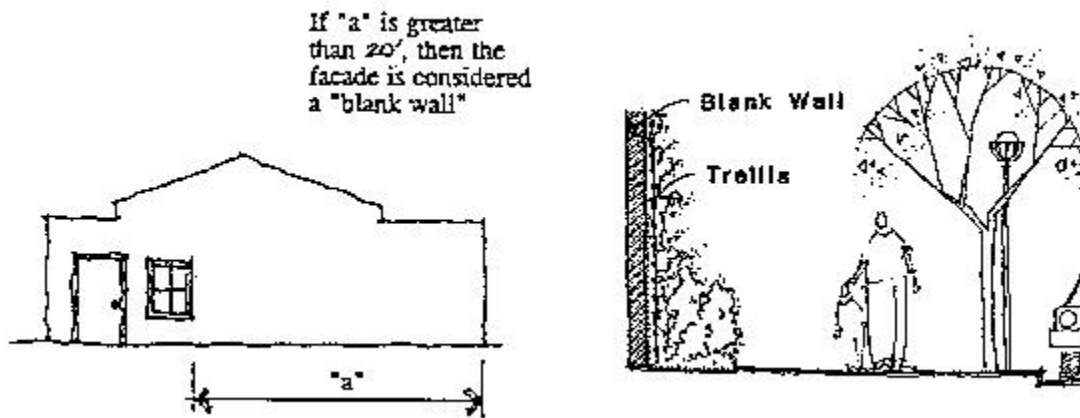
Figure 15.35.1051E – Windows shall cover at least sixty percent (60%) of the ground floor.

D. 15.35.530 Treatment of Blank Walls

The definition of a “blank wall” is any wall or portion of a wall that is located within forty (40) feet of a street or pedestrian-only corridor and is without a ground level window, door or facade opening along any street-facing facade section of twenty (20) feet in length or more.

- A1. Where blank wall sections are unavoidable due to the requirements of a particular land use or structural needs, they shall not exceed a length of fifty (50) feet, or twenty percent (20%) of the length of the street-facing facade, whichever is less.
- B2. Blank wall sections of allowed lengths shall receive one (1) or more of the following special design treatments up to at least the finished ceiling height of the first floor building space in order to increase pedestrian comfort and interest:
- 1a. Install vertical trellis in front of the wall with climbing vines or other plant materials over at least thirty percent (30%) of the blank wall surface;

- 2b. Provide a decorative masonry pattern, or other architectural feature as approved by the Director of Community and Economic Development, over at least thirty percent (30%) of the blank wall surface; and/or
- 3c. Employ small setbacks, projections, indentations, or intervals of material change to break up the wall's surface.



[Figure 15.35.1051D - Blank Wall Dimensions and Design Treatments](#)

E. [15.35.570](#) Pedestrian Weather Protection Along Building Facades

1. [15.35.570](#) Building facades with ground floor retail/commercial or service uses shall be designed to provide for pedestrian weather protection through the use of awnings, canopies, colonnades, marquees, or building overhangs. Pedestrian weather protection structures shall extend along at least the length of the street-facing facade with the ground floor retail/commercial or service use.
- 2A. [15.35.570](#) Pedestrian weather protection structures shall extend a minimum of four (4) feet out from the building facade. The maximum horizontal projection from the surface of the building shall be eight (8) feet or seventy-five percent (75%) of the distance to the curb face, whichever is less. Pedestrian weather protection structures shall be architecturally integrated with the ground level design of the building to which it is attached.
- 3B. [15.35.570](#) The minimum height of pedestrian weather protection structures shall be eight (8) feet and six (6) inches above the sidewalk surface. Maintain a horizontal consistency by aligning the bottom edge of weather protection structures with those on adjacent buildings. Where the grade is sloping, maintain the average height above grade of adjacent weather protection structures.

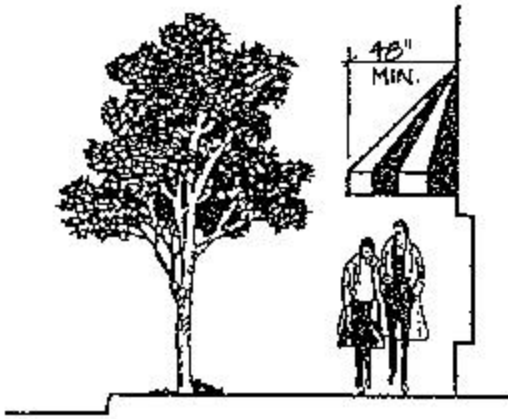


Figure 15.35.1053E – Pedestrian weather protection shall extend at least four (4) feet from the building façade.

~~Retail/commercial/service space The applicable ground floor area shall extend in depth a minimum of twenty (20) feet from the exterior parking structure facade; provided, that the minimum required may be averaged, with no depth less than fifteen (15) feet.~~

F. Building Colors and Materials

1. Quality, durable materials shall be used in building design. Materials that have a track record of installation difficulties or lack of durability shall be subject to provision of warranty information from manufacturers and installers. Building materials with a history of problems with installation and rapid decay may be disallowed.
2. Predominant materials should include any combination of brick, wood, stone, and/or tinted/textured concrete
3. The following materials and features are prohibited:
 - a. Mirror glass or other highly reflective materials;
 - b. Corrugated fiberglass;
 - c. Chain link fencing; and
 - d. Bare or painted concrete as a predominant façade treatment.



Figure 15.35.1051F(1-2) – Example of desirable building materials – combination of brick and stone.



Figure 15.35.1051F(3) – Example of undesirable building materials – bare concrete as predominate façade treatment.

G. ~~15.35.550~~ Rooftop Equipment

Building rooftops shall be designed to effectively screen mechanical equipment from street-level view through one (1) or more of the following methods:

- 1A. _____ A concealing roof line;
- 2B. _____ A terraced facade;
- 3C. _____ A screening wall or grillwork directly surrounding the equipment;
- 4D. _____ Sufficient setback from the facade edge to be concealed from ground level view. (Ord. 99-1050 § 8)

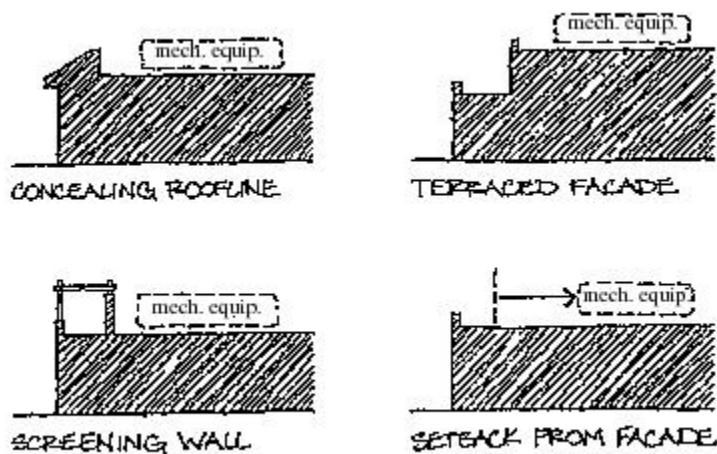


Figure 15.35.1051G – Rooftop equipment shall be screened from street level view through on the methods shown.

15.35. 105215.35.540 Non-Parking Structures Roof Lines

This subsection shall only apply to non-parking structures associated with a commercial park-and-fly parking development.

In order to provide a visual terminus to the tops of City Center buildings and soften rectilinear forms, non-parking structure roof designs must conform to one (1) of the following options:

- A. Roof Line with Architectural Focal Point. A roof line focal point refers to a prominent rooftop feature such as a peak, tower, gable, dome, barrel vault or roof line trellis structure.
- B. Roof Line Variation. The roof line articulated through a variation or step in roof height or detail, such as:

1. Projecting Cornice: Roof line articulated through a variation or step in cornice height or detail. Cornices must be located at or near the top of the wall or parapet.
 2. Articulated Parapet: Roof line parapets shall incorporate angled, curved or stepped detail elements.
- C. Pitched Roof or Full Mansard. A roof with angled edges, with or without a defined ridge line and extended eaves.
- D. Terraced Roof. A roof line incorporating setbacks for balconies, roof gardens, or patios. (Ord. 99-1050 § 8)

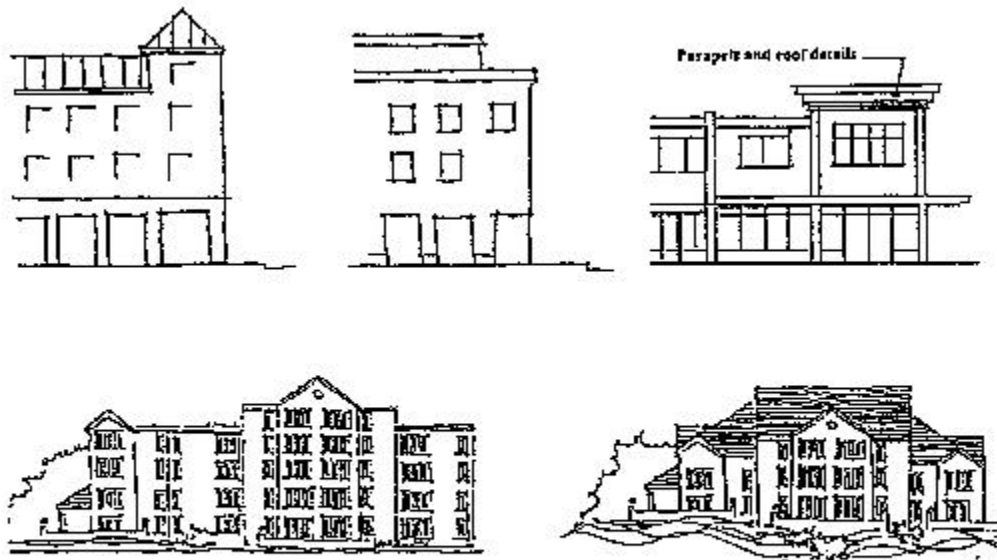


Figure 15.35.1052A – Examples of non-parking structure roof designs.

15.35.1053 910 Parking Structures Design

The following parking structure design standards shall be in addition to or, in some cases, supersede applicable design standards in other sections in this chapter.

This subsection shall only apply to parking structures associated with commercial park-and-fly parking developments.

A. General

1. Parking structures shall conform to the standards set forth in SMC 15.15.100, Off-Street Parking Plan Design, and Crime Prevention through Environmental Design (CPTED) standards as set forth in SMC 17.28, Parking Structures.

2A. _____ Parking decks ~~shall~~should be flat to the maximum extent possible to increase visibility. where feasible.

3. _____ At a minimum, a majority of both the ground floor and top parking decks shall be required to be flat, as opposed to continuously ramping.

4. _____ Methods such as long-span construction and high ceilings shall be used to encourage openness.

~~B. _____ External elevator towers and stair wells shall be open to public view, or enclosed with transparent glazing.~~

5C. _____ Lighting on and/or within multi-level parking structures shall be screened, hooded or otherwise limited in illumination area so as to minimize excessive “light throw” to off-site areas.

B. _____ Roofs/Top Floors

~~1.~~

~~D.~~ _____ Parking structure roofs/top floors ~~wall designs~~ must conform to one (1) or more of the following options:

1a1. _____ ~~Top Floor Wall with Architectural Focal Point.~~ A roof/top floor wall focal point ~~refers to a prominent wall edge feature~~ such as a peak, glazed elevator and/or stair tower, or top floor line trellis structure.

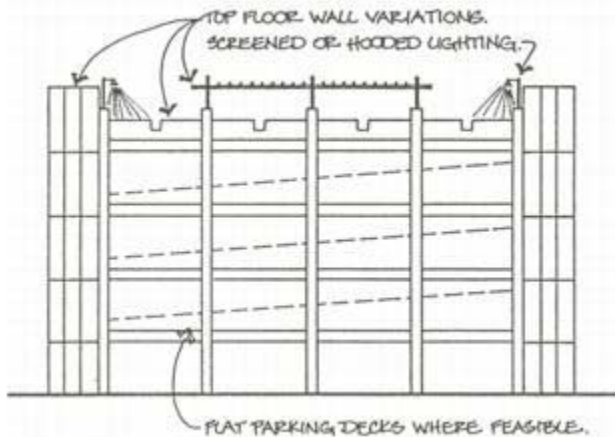
~~2b2.~~ _____ ~~Top Floor Wall Line Variation.~~

a. _____ Projecting Cornice: Roof/tTop floor wall line articulated through a variation or step in cornice height or detail. Cornices must be located at or near the top of the wall or parapet.

3eb. _____ Articulated Parapet: Roof/tTop floor wall line parapets shall incorporate angled, curved or stepped detail elements. (Ord. 99-1050 § 8)



Figure 15.35.1053A – Glazed stair tower as a roof/top floor focal point.



C.C. 15.35.930 Parking Area Screening ~~Minimizing Views Into the Parking Structure Interior~~

~~Facades of parking structures facing a public street or pedestrian way as defined by SMC 15.35.220 shall be designed without continuous horizontal parking floor openings.~~

1. ~~B. Ground Floor Parking Areas. Any portion of a parking structure g~~Ground floor ~~with exposed parking areas adjacent to~~ facing a public or private street or pedestrian way shall be completely enclosed or wholly screened ~~minimize views into the parking structure interior through~~ through any combination of walls, grilles, or transparent or opaque glazing. ~~one (1) or more of the following methods which are in addition to the above facade landscaping strip:~~

~~a1.~~

~~Decorative trellis work and/or screening as architectural elements on the parking structure facade, without compromising the open parking structure requirements of the Building Code; and/or~~

~~2. Glass window display cases incorporated into ground floor walls built between two (2) structural pillars. Glass window display cases shall be at least two (2) feet deep, begin twelve (12) to thirty (30) inches above the finished grade of the sidewalk, and cover at least sixty percent (60%) of the area between two (2) pillars.~~

~~C. Upon conversion of portions of a parking structure ground floor to a retail/commercial use, the Director of Planning and Community Development may approve the removal of initially installed ground floor screening material in order to allow maximum visibility and access to the converted portions of the parking structure ground floor.~~

~~2D. Upper Floor Parking Areas. In addition to the above, minimize views into the upper floors of parking parking structures areas facing a street or pedestrian way shall be screened through one (1) or more of the following methods: incorporating a combination of decorative grilles, railings, the design of window like openings, and/or other screening materials aesthetically compatible with the building design.~~

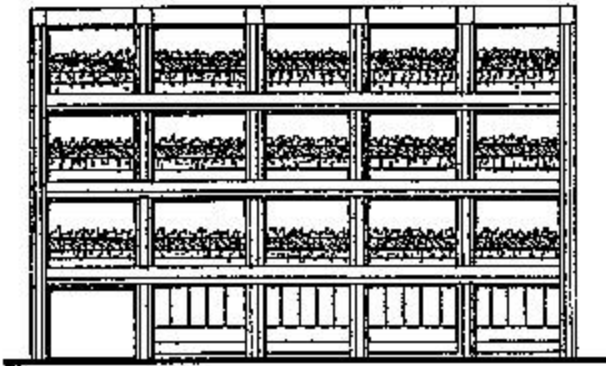
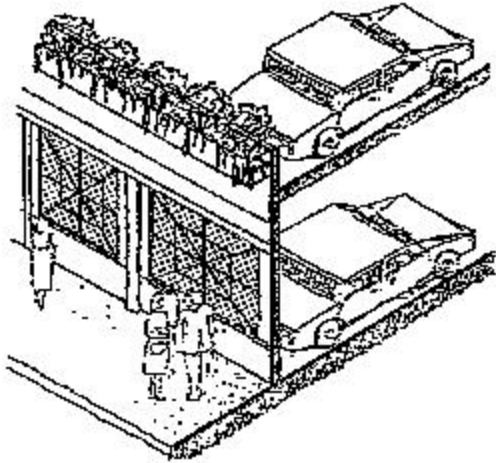
~~1. The use of planters integrated into the upper floors of parking structure facade design;~~

~~b2. Decorative trellis work and/or screening as architectural elements on the parking structure upper floor facades; and/or~~

~~e3. Upper parking floors designed as a pattern of window like openings on the parking structure facade. (Ord. 04 1008 § 4; Ord. 99 1050 § 8)~~



Figure 15.35.1053B – Example of upper floor screening using window like openings and other screening materials.



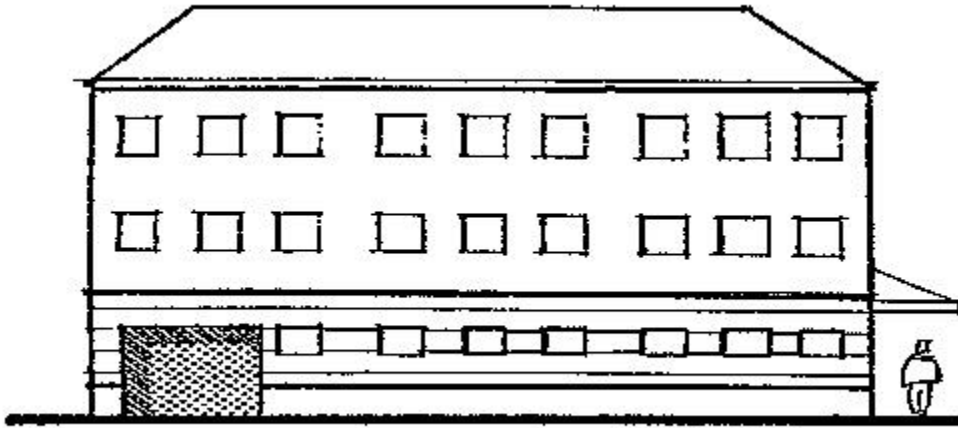
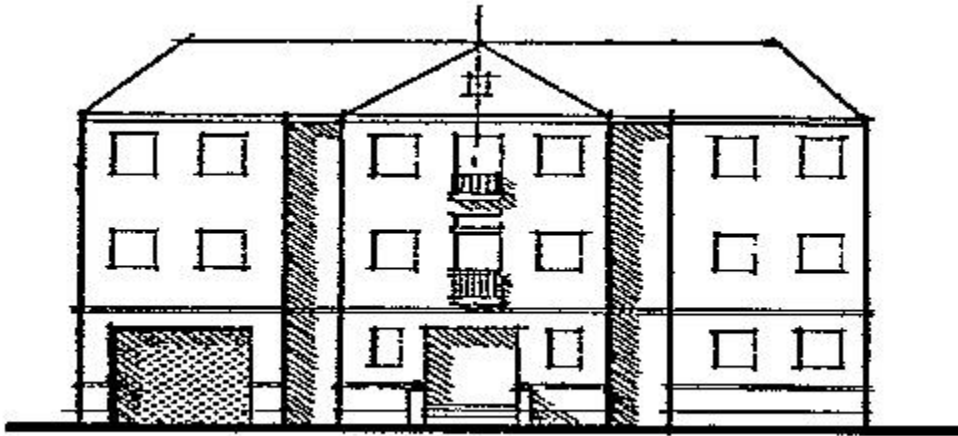
D.

~~D. 15.35.940 Vehicle Entrances~~ Parking Floors Located Under or Within Buildings

A. — Vehicle entrances ~~Parking located under or within buildings~~ shall be subordinate ~~the garage entrance~~ to the pedestrian entrances in terms of prominence on the street, location and design emphasis.

B. — Parking at grade under a building shall be completely enclosed within the building or wholly screened through any combination of walls, decorative grilles, or trellis work with landscaping. (~~Ord. 99 1050 § 8~~)

Figure 15.35.1053C – Vehicle entrance.



Chapter 17.28 PARKING STRUCTURES

(Revised sections include: 17.28.010, 17.28.030)

Sections:

- 17.28.010 Application
- 17.28.020 Purpose
- 17.28.030 Parking Structure Standards
- 17.28.040 Illuminance Standards

17.28.010 Application

Parking structure standards apply to covered and enclosed facilities intended for use by the general public, for those used by residents, customers, and employees of apartment buildings or commercial developments. They are not intended to apply to parking structures used exclusively for repair or storage of commercial vehicles, ~~or where attendants park vehicles~~. (Ord. 03-1033 § 11)

17.28.020 Purpose

- A. This chapter establishes CPTED design and lighting standards for parking structures. These standards include interior and exterior CPTED strategies for the reasonably safe movement of vehicular and pedestrian traffic in parking structures, the enhancement of personal security and deterrence of vandalism, theft and assault while conserving energy and minimizing maintenance.
- B. Parking structures comprise a large volume of space with relatively low levels of activity. Violent crime is more likely to occur in a parking structure than in other commercial facilities. Many parking structures lack CPTED features because property owners and architects are not familiar with basic principles of design concepts for crime prevention. Although CPTED principles can be readily incorporated into parking structures at design and construction stages, it is often difficult and expensive to upgrade security at a later date. Incorporating CPTED can significantly reduce the fear and risk of crime.
- C. Parking lots and parking structures have vehicular speeds that are much lower than roadways. More importantly, the primary purpose of lighting here is to benefit the pedestrian.
- D. The illuminance requirements of a parking facility are affected by the layout, operation and vehicular traffic safety issues, plus the visibility and security needs of pedestrians walking to and from their vehicles. Lighting for parking structures should provide not only the recommended minimum illuminance levels but also good color rendition, uniformity, and minimal glare. (Ord. 03-1033 § 11)

17.28.030 Parking Structure Standards

Parking structures shall meet the recommendations of the IESNA Handbook. At a minimum, the following standards shall be used in the construction of parking structures. It is required that the developer comply with Lighting for Parking Facilities, Recommended Practices RP-20-98 and Chapter 22, Roadway Lighting as delineated in the IESNA Handbook, 9th Edition.

A. Stairs and Elevators

1. For security reasons, it is recommended to locate ~~S~~stairs and elevators shall be adjacent to a street, and designed so that ~~where~~ the lobbies are visible from the street.

2. Elevator towers and stair wells shall be open to public view to the maximum extent possible. If an enclosure is necessary for public safety or for weather protection, it shall be enclosed with transparent glazing.

3. Elevators shall be located at the exterior of the building and oriented so that the elevator lobby is visible from street at each level.

4. The back of elevator cabs and shaft shall be made of glass to allow for maximum surveillance.

~~5C.~~ _____ Potential hiding places below stairs shall be closed off. If used for storage, such areas shall be secured with doors and locks.

B. Pedestrian walkways shall be elevated similar to a speed hump/bump in order to allow pedestrians greater visibility and to assist in slowing cars when in the vicinity of pedestrians.

~~DC.~~ _____ Directional arrows indicating stairs, elevators and exits shall be painted on walls or indicated with illuminated signs.

~~DE.~~ _____ Remote exterior stairway doors shall be equipped with one-way locks allowing people to exit but not to enter the facility at those locations.

~~EF.~~ _____ Pedestrian entrances shall be concentrated to bring all pedestrians through one (1) portal, which improves the ability to see and be seen by others.

~~FG.~~ _____ Any ground-level pedestrian exits that open into nonsecure areas shall be emergency exits only and fitted with self-locking doors.

~~GH.~~ _____ Active security measures such as emergency phones and closed circuit television (CCTV) are recommended.

- H. _____ Fencing shall not be used unless security dictates. If necessary, a six (6) foot high black-coated chain link fence is recommended. Openings in the fence should be located carefully, with only a minimum number of openings allowing cars and pedestrians to enter and leave.
- I. Landscape plantings shall not be located within three (3) feet of the interior curbing of the perimeter of the parking lot. (Ord. 03-1033 § 11)

17.28.040 Illuminance Standards

- A. Lighting shall be positioned to illuminate both horizontally and vertically. Illumination must reach into the edges of the parking stalls rather than just the driving aisles to minimize dark hiding places between cars.
- B. To minimize glare, lights shall be positioned between parked vehicles rather than in the center of drive aisles.
- C. Cutoff luminaries shall be required on all lighting mounted higher than twelve (12) feet above the floor.
- D. Interior parking structure walls, beam faces and underside of floor slabs shall be stained or painted a light color, preferably white or light blue, to increase the general brightness in parking structures. Anti-graffiti coating added to walls should be considered.
- E. _____—The illuminance requirements for parking structures shall be in accordance with the standards in Table 17.28.030a. The illuminance shall be measured, or calculated, on an unobstructed floor. The maximum and minimum values are maintained illuminance levels; this occurs just prior to lamp replacement and luminaire cleaning. Vertical illuminances of objects such as walls and columns shall be equal to the horizontal values given in Table 17.28.030a. These vertical values shall be for a location measured at a height of five (5) feet above the pavement.

TABLE 17.28.030a: Maintained Illuminance Values for Parking Garages

	Minimum Horizontal Illumination (footcandles)	Maximum/Minimum Horizontal Uniformity Ratio*	Minimum Vertical Illumination (footcandles)
Basic	1.0	10:1	0.5
Ramps			
Day**	2.0	10:1	1.0
Night***	1.0	10:1	0.5
Entrances			
Day	50	No Requirement	25
Night	1.0	10:1	0.5
Stairways	2.0	No Requirement	1.0

* The measure of uniformity in this table is the ratio of maximum light level to minimum light level.

** Day is the time after sunrise and before sunset.

*** Night is the time after sunset and before sunrise.

(Ord. 03-1033 § 11)

Attachment 2. Plans and Policies Review

This plans and policies review discusses the City of SeaTac Comprehensive Plan, including the Land Use Element and the City Center Plan. This review identifies adopted goals and policies that are applicable to the proposal and a short discussion of consistency of the proposal with these goals and policies.

Comprehensive Plan

The City's Comprehensive Plan contains the City's 20-year vision for the community and includes the mandated elements of land use, housing, capital facilities, utilities, transportation, economic development, and parks and recreation. The City of SeaTac has elected to include in its Comprehensive Plan several optional subarea plans, including the City Center Plan. The Comprehensive Plan notes that the subarea plans fit within the vision articulated by the SeaTac Comprehensive Plan and, as such, are components of the SeaTac Comprehensive Plan.

Pertinent goals and policies from the SeaTac Comprehensive Plan and City Center Plan are excerpted and discussed below.

Land Use Element

Policy 1.1D. Encourage the creation of a "town center" or central business district within the Urban Center's boundaries.

Policy 1.1G. Provide a network of connected streets in the three HCT districts and elsewhere through the Urban Center to facilitate pedestrian circulation and transit accessibility.

Policy 1.3D. Encourage mixed use projects, especially those with a retail/office/residential component, in designated areas of the City.

Policy 1.3E. Encourage new and redeveloped buildings within SeaTac's Urban Center to be well designed.

Discussion. Through measures that promote mixed use, pedestrian-scaled design features, parking screening, enhanced building facades and other similar measures, the proposal is consistent with Policies 1.1D, 1.3D, and 1.3E.

Regarding Policy 1.1G, the Comprehensive Plan states that the physical layout of the Urban Center, including its street and sidewalk network, block size and configurations, is perhaps the most crucial determinant of its pedestrian and transit accessibility. Discussion of this policy further states that to convert the area to a form that is more pedestrian and transit friendly will require that the current average block size be reduced through the addition of new streets as development occurs. Existing regulations in SMC 15.35.210 are intended to implement the intent of a reduced block size through the addition of new streets as development occurs. The regulations establish right-of-way standards, a standard of block faces that are no more than 400 feet in length on a side, ground level retail requirements and public art requirements. The proposal would eliminate these requirements and replace them with SMC Title 11, Streets, Sidewalks and Public Thoroughfares. Because SMC Title 11 does not promote increased connectivity in City Center, this change is not consistent with Policy 1.1G.

Goal 6.2 To provide a well-designed, pedestrian-friendly and community-oriented environment in the Urban Center.

Policy 6.1F Open Space Provisions. Provide for open space in commercial districts and business park developments.

Policy 6.2H Pockets of Public Space. Establish a variety of public spaces throughout the Urban Center.

Policy 6.2L Parking Facilities. Ensure that the appearance of parking structures makes a positive contribution to the image of the Urban Center.

Policies 6.2P through 6.2W address a variety of design principles, including bulk (base, middle, top), scale, orientation, roof forms, rooftop equipment, continuity and variety, public amenities and through block connections.

Discussion. The proposed code amendments are generally consistent with cited applicable Community Image goals and policies. The proposal specifically includes a number of revised design standards with the intent of promoting buildings that contribute to the overall character of the City Center, including measures to enhance the street wall, increased requirements for vertical and horizontal articulation, architectural requirements to ensure high quality building materials and focal points along the front façade and other measures.

Regarding Policies 6.1F and 6.2H, the proposal would reduce the amount of required open space from the existing requirement of 10% of the site area, located anywhere on the site, to 5% of site area, primarily located in the front of the site. This change is based on the recognition that the 10% requirement is not feasible for many sites within City Center. Reduction to the 5% requirement and focusing open space in the front yard is intended to ensure that the open space amenity is still provided and the visual/public benefit of open space is maximized, while recognizing the reality of site and economic constraints for development. The proposal is consistent with the pertinent Comprehensive Plan policies.

Economic Vitality Element

Goal 7.0 To create an environment that will strengthen the economic vitality of businesses within the City of SeaTac.

Discussion. A major purpose of the revised park and fly standards is to promote economic vitality. Revised standards were developed with input from parking providers and have been extensively reviewed over a two year period by the Ad Hoc Committee, which included a variety of representatives and interests. Because the proposal represents the perspectives and interests of parking providers, it is consistent with Economic Vitality Element Goal 7.0.

City Center Plan

The City Center Plan, adopted in December 1999 and amended in November 2010, describes an urban design vision and development approach for creating a City Center in the City of SeaTac over the next 20 years. The primary objectives of the City Center Plan are to focus growth in the City Center and promote integrated development, pedestrian-oriented design, diversity of uses within close proximity, link open spaces to the residential areas, and create a Civic Center focal point for community identity.

Goal 2. To create a new "Main Street" through the City Center that encourages mixed use and pedestrian-friendly development.

Policy LU-2A. Encourage high quality mixed-use development along "Main Street."

Discussion. The proposal is generally consistent with Land Use Goal 2 and Policy LU-2A. Specifically, the following elements of the proposal promote the land use vision:

- Promoting a mix of uses by increasing the minimum amount of ground floor commercial space to be built when park-and-fly projects are constructed adjacent to the street;
- Enhancing pedestrian-oriented design by addressing building scale, façade treatments, building materials and ground floor use requirements;
- Concealing parking by increasing requirements for the screening of parking including design treatments for the first two stories of garages in order that they have the appearance of commercial buildings, the complete screening of parking at grade and screening treatments for parking in upper levels.
- Contributing to distinctive buildings and dramatic skylines through encouraging enhanced building facades and architectural focal points, especially for buildings located at intersections.
- Promoting the pedestrian environment by requiring the construction of significant on-site publicly accessible open space to activate building frontage while reducing the minimum amount of open space required per development site.

Transportation/Roads

Goal 1. To provide safe and efficient vehicular access to, from and through the City Center.

Policy T-1A. Create a new "Main Street" along 32nd Avenue South and provide adequate connections to existing and future City Center streets.

Policy T-1B. Ensure adequate vehicular access from the City Center to International Boulevard and other local and regional facilities.

Policy T-1C. Create a connected network of public and private collector streets to promote pedestrian access and provide vehicular circulation.

Discussion. The City Center Plan states that access to City Center is very limited by the existing sparse street pattern and the single regional arterial street (International Boulevard). The Plan identifies a need to identify a pedestrian and vehicular circulation system to promote denser development and transform the Center into a focal point for the City. Existing regulations in SMC 15.35.210 are intended to implement the transportation goals and policies in the City Center Plan. The regulations establish right-of-way standards, a standard of block faces that are no more than 400 feet in length on a side and requirements for the interconnectivity of adjacent parcels. The proposal would eliminate these requirements and replace them with SMC Title 11, Streets, Sidewalks and Public Thoroughfares. Because SMC Title 11 does not promote increased connectivity in City Center, this change is not consistent with transportation Goal 1 and supporting policies.

Goal 1. To create a sense of place and community and to enhance the City's image.

Policy CI-1A. Improve the City Center's image through high-quality design standards and other features that enhance the City's image.

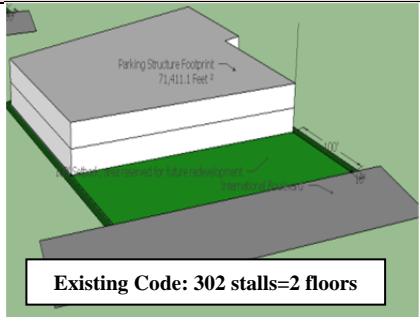
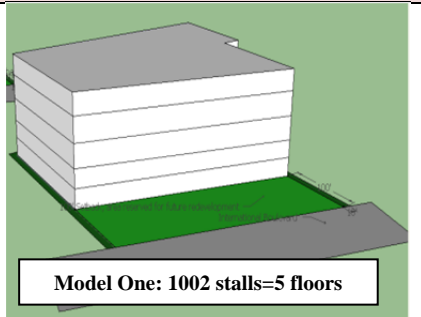
Discussion. The proposed code amendments are generally consistent with Goal 1 and Policy CI-1A. The proposal specifically includes a number of revised design standards with the intent of promoting buildings that contribute to the overall character of the City Center, including:

- Requiring one story buildings to have the appearance of two story buildings to enhance the street wall;
- Increasing requirements for vertical and horizontal articulation and the screening of parking at grade and upper levels to reduce the bulk and scale of projects;
- Requiring the first two stories of buildings fronting International Boulevard to have the appearance of a commercial building to create a more human scale of development;
- The addition of design treatments within the first two stories of structures and requirements for high quality building materials;
- The prohibition of building materials that might detract from the pedestrian-oriented environment such as reflective materials, corrugated fiberglass and chain link fencing; and,
- Requiring architectural focal points along the front façade and architectural expression on the corners of buildings.

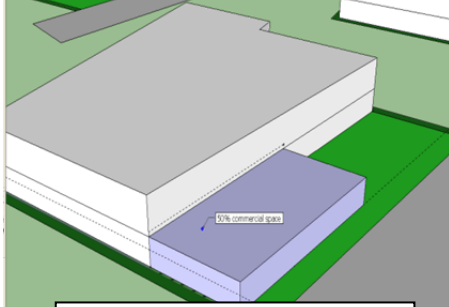
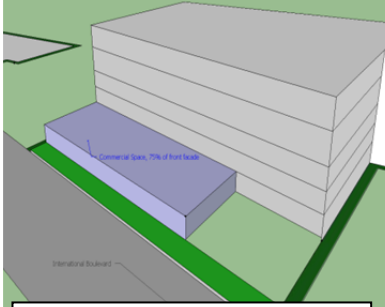
ATTACHMENT 3. EXISTING DEVELOPMENT EXAMPLES

The tables below compare the existing code and the proposed Model One code to ascertain any differences in the number of parking stalls and commercial space permitted as a result of the change in the base stall count (from 300 increased to 1000) and the changes to the commercial space requirements. Also presented is a comparison of the WallyPark and MasterPark structured park-and-fly developments.

These tables are for general reference only.

BUILDING TYPE ONE	Existing/Current Code	Proposed Model One
Using the existing MasterPark lot dimensions and size; ASSUME 320 sf per parking structure stall (incorporates driving lanes and ramps)	100' setback to parking structure with 50% ground floor commercial to be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for park and fly office uses)	<i>The amount of commercial space is reduced due to economic considerations of accommodating a 100' setback.</i> 100' setback to parking structure with 400 sf commercial space (which can be used for park and fly office uses)
Total Lot Size	111,184 sf	111,184 sf
<ul style="list-style-type: none"> Less Total Parking Footprint 	71,411 sf	71,411 sf
<ul style="list-style-type: none"> Less Landscaping: side and rear 10' 	10,133 sf	10,133 sf
<ul style="list-style-type: none"> Less 100' setback and open space 	100' setback 29,639 sf (this includes 10% open space area of 11,118 sf)	100' setback 29,639 sf (this includes 5% open space area of 5,559 sf)
Commercial Space Required	400 sf	400 sf
Base stalls	300 spaces	1000 spaces
Commercial Space Parking Required (1: 250 sf)	2 spaces	2 spaces
Total Number of parking stalls Permitted	302	1002
Total stalls per floor of parking	223	223
Total Floors of parking permitted	1.4 ~2 floors	4.5 ~5 floors
NUMBER OF EXTRA PARKING STALLS	-	700 STALLS
		

BUILDING TYPE ONE (cont.)	Existing/Current Code	Proposed Model One
CONCLUSIONS		<ul style="list-style-type: none"> The additional 700 base stalls as proposed in Model 1 equates to an additional 3 floors of structured parking based on the configured lot used in this example.
<p>Assume developer wants to construct 7 floors of park and fly. Applying the bonus program, the following amount of commercial space will be required to be constructed to achieve 7 floors of park and fly stalls.</p>	<p>Bonus Ratio: 1 stall per 250 sf of commercial space.</p> <p>Parking Structure Footprint: 71,411 sf Divide by: 320 sf per parking stall Multiply by 7 stories <i>EQUALS 1,562 parking stalls in total can be accommodated in 7 stories</i> Less: 300 base stalls <u>Developer needs to apply bonus program to gain an additional: 1262 stalls</u> Additional Commercial Space Required: 315,500 sf (note: an additional 1,262 commercial parking stalls will also be required)</p> <p>TOTAL STALLS: 2,824 stalls TOTAL FLOORS: 13 floors</p>	<p>Bonus Ratio: 1 stall per 25 sf of commercial space.</p> <p>Parking Structure Footprint: 71,411 sf Divide by: 320 sf per parking stall Multiply by 7 stories <i>EQUALS 1,562 parking stalls in total can be accommodated in 7 stories</i> Less: 1000 base stalls <u>Developer needs to apply bonus program to gain an additional: 562 stalls</u> Additional Commercial Space Required: 14,050 sf (note: an additional 57 commercial parking stalls will also be required)</p> <p>TOTAL STALLS: 1,619 stalls TOTAL FLOORS: 8 floors</p>
CONCLUSIONS	<ul style="list-style-type: none"> Under existing code, without utilizing the bonus program, a developer can construct a park and fly structure of 2 stories on the existing Master Park lot. If a developer wants to construct 7 stories of park and fly, applying the commercial space bonus incentive of 1:250, a developer actually needs to construct 13 stories (7 stories park and fly & 6 stories commercial parking) of parking and provide 315,500 sf of commercial space as per the bonus program 	<ul style="list-style-type: none"> Inaccordance with the proposed code, without utilizing the bonus program, a developer can construct a park and fly structure of 5 stories on the existing Master Park lot. If a developer wants to construct 7 stories of park and fly, applying the commercial space bonus incentive of 1:25, a developer actually needs to construct 8 stories (7 stories park and fly & 1 story commercial parking) of parking and provide 14,050 sf of commercial space as per the bonus program

BUILDING TYPE THREE	Existing/Current Code	Proposed Model One
<p>Using the MasterPark lot; ASSUME 320 sf per parking structure stall (incorporates driving lanes and ramps)</p>	<p>0'-20' setback (for at least 50% of the buildings front façade) to commercial development; 100' setback to parking structure; 50% ground floor commercial be provided as, or convertible to, retail/commercial/service space. 400 square feet of ground floor retail/commercial/service space must be provided at occupancy (not to be used for park and fly office uses)</p>	<p><i>The setback to the commercial space is reduced and the amount of commercial space required has been increased.</i></p> <p>0'-20' setback to commercial development; 75' setback to parking structure; 75% ground floor commercial available at time of occupancy</p>
<p>Total Lot Size</p>	<p>111,184 sf</p>	<p>111,184 sf</p>
<ul style="list-style-type: none"> Less Total Parking Footprint 	<p>71,411 sf</p>	<p>71,724 sf</p>
<ul style="list-style-type: none"> Less Landscaping: side and rear 10' 	<p>10,133 sf</p>	<p>10,133 sf</p>
<ul style="list-style-type: none"> Less 20' front setback (note: open space is not included in these calculations) 	<p>6,174 sf</p>	<p>6,174 sf</p>
<p>Commercial Space Required</p>	<p>Assume 50% ground floor commercial 11,706 sf</p>	<p>16,683 sf</p>
<p>Base stalls</p>	<p>300 spaces</p>	<p>1000 spaces</p>
<p>Commercial Space Parking Required (1: 250 sf)</p>	<p>47 spaces</p>	<p>66 spaces</p>
<p>Total Number of parking stalls Permitted</p>	<p>347 = 2 floors of parking</p>	<p>1066 spaces = 5 floors of parking</p>
<p>Total stalls per floor of parking</p>	<p>223</p>	<p>224</p>
<p>Total Floors of parking permitted</p>	<p>1.5~2 floors</p>	<p>4.75 ~5 floors</p>
<p><i>NUMBER OF EXTRA PARKING STALLS</i></p>		<p>719 spaces</p>
	 <p>Existing Code: 342 stalls=2 floors 50% Commercial Space</p>	 <p>Model One: 1066 stalls=5 floors 75% Commercial Space</p>
<p>CONCLUSION</p>	<p>An increase in the amount of ground floor commercial space from 50% to 75% and an increase in the number of base stalls from 300 stalls to 1000 stalls results in a parking structure being an additional 3 stories higher than what is permitted under the existing code.</p>	

WallyPark and MasterPark Park-and-Fly Garage Comparison Tables

The two tables below identify the potential differences in the WallyPark and MasterPark park-and-fly mixed use developments if each were constructed under the originally adopted code provisions, current code provisions, proposed Model One code, and compared to what was actually constructed. The tables are based on approximate numbers only and do not consider potential site constraints. The purpose of the tables is to demonstrate the range in the number of parking stalls that could be permitted using different base stall and bonus incentive programs:

WALLY PARK	As per First Adopted City Center Code: Dec , 1999 (prior to any code changes)	As Constructed (Development Agreement)	Existing/Current Code	Proposed Model One Code
Base Stall Allowance	1200 base stalls <i>Standard: 300 base stalls per legal lot 4 lots in total</i>	900 base stalls <i>(Development Agreement)</i>	300 base stalls <i>(1 parking structure per development site: code change adopted 11 Jan, 2005)</i>	1000 base stalls <i>(1 parking structure per development site)</i>
Code Required Commercial Space	400 sf	400 sf	400 sf	400 sf
BONUS STALL CALCULATIONS				
Bonus stalls from Commercial Space: 16,136 sf (constructed)	1:25 sf 646 stalls	1:25 sf 646 stalls	1:250 sf (adopted 25 March, 2008) 65 stalls	1:25 sf 616 stalls
Bonus stalls from Open Space: 13,087 sf	1:150 sf 88 stalls	1:150 sf 88 stalls	1:150 sf 88 stalls	1: 100 sf 130 stalls
One water feature	60 stalls	60 stalls	60 stalls	60 stalls
TOTAL PARK AND FLY STALLS ALLOWED	1964 STALLS	1694 STALLS	513 STALLS	1806 STALLS

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Attachment 3

MASTER PARK	As per First Adopted City Center Code: Dec, 1999 (prior to any code changes)	As Constructed (Development Agreement)	Existing/Current Code	Proposed Model One Code
Base Stall Allowance	3000 base stalls <i>Standard: 300 base 10 lots in total</i>	600 base stalls	300 base stalls <i>Standard: 1 parking structure per development site (code change adopted 11 Jan, 2005). 10 lots in total</i>	1000 base stalls
Code Required Commercial Space	400	400	400	400
BONUS STALL CALCULATIONS				
Bonus stalls from Commercial Space: 5,675 sf (constructed)	1:25 sf 227 stalls	1:25 sf 227 stalls	1:250 sf (adopted 25 March, 2008) 23 stalls	1:25 sf 227 stalls
Bonus stalls from Open Space: 22,838 sf	1:150 sf 152 stalls	1:150 sf 152 stalls	1:150 sf 152 stalls	1: 100 sf 229 stalls
One water feature	60 stalls	60 stalls	60 stalls	60 stalls
TOTAL PARK AND FLY STALLS ALLOWED	3439 STALLS	1039 STALLS	535 STALLS	1516 STALLS